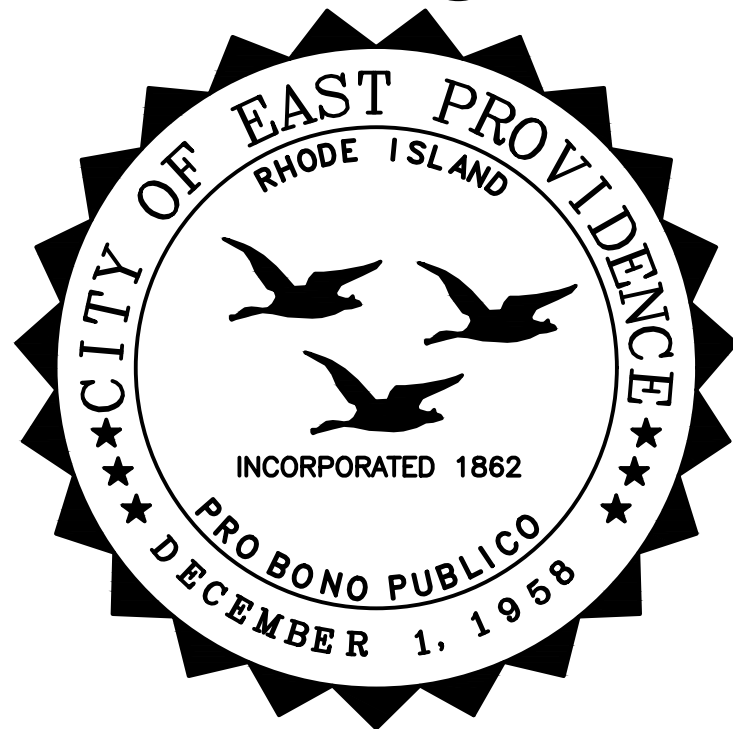
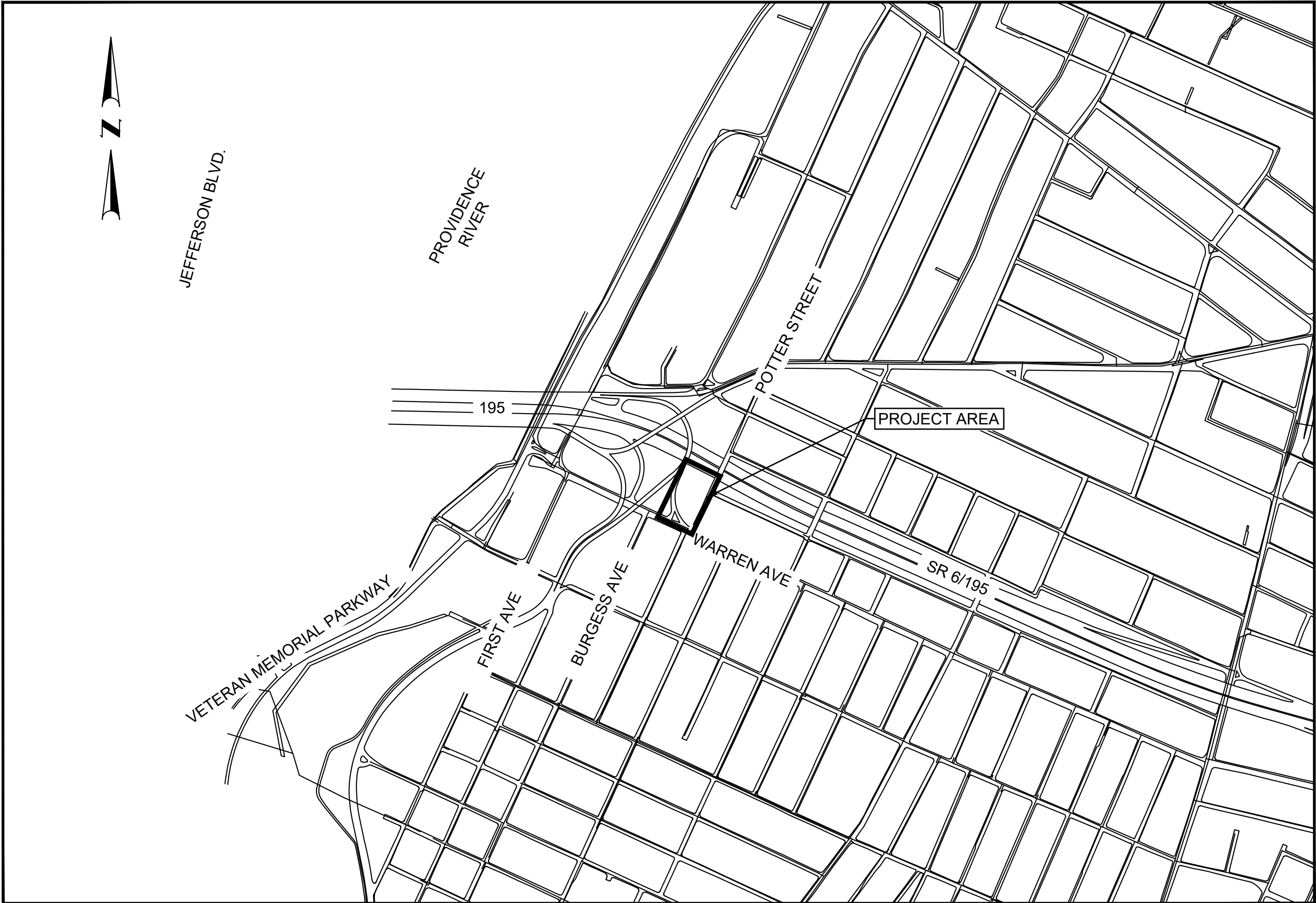


City of East Providence
Department of Public Works
Engineering Division



PROPOSED PARKING AREA
Potter Street at Warren Avenue
East Providence, Rhode Island
VOLUME 1 OF 2

August 2023



R.I. STANDARD SPECIFICATIONS AND STANDARD DETAILS

SPECIFICATIONS TO GOVERN THIS PROJECT ARE THE R.I. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, DECEMBER 2022, WITH ALL REVISIONS AND THE STATE AND FEDERAL SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

STANDARD DETAILS FOR THIS PROJECT ARE R.I. STANDARD DETAILS, 1998 EDITION, WITH ALL REVISIONS.

ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), 2009 EDITION, INCLUDING ALL REVISIONS.

LAYOUT PLAN
SCALE: 1" = 500'

BASE OF LEVELS
VERTICAL DATUM: NAVD 88

HORIZONTAL DATUM: RHODE ISLAND STATE PLANE, NAD 83



Number of Sheet 1
Total Sheets 12



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GENERAL NOTES:

- ANY DAMAGE TO EXISTING PAVEMENT, BRIDGES, DRAINAGE STRUCTURES, DRAINAGE PIPES, INFILTRATION AREAS, ROADSIDE, CONDUIT, SIDEWALK, FENCES, ETC., CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.
- THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL AS FAR AWAY AS POSSIBLE FROM THE EDGE OF THE TRAVEL LANE SO AS NOT TO CAUSE A SAFETY HAZARD, IN ACCORDANCE WITH SECTION 106.05 OF THE R.I.D.O.T. STANDARD SPECIFICATION, LATEST EDITION. EQUIPMENT AND MATERIAL SHALL NOT BE STORED IN AREAS DESIGNATED FOR STORMWATER INFILTRATION OR OUTSIDE THE L.O.D. WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE EXISTING CONDITIONS ARE NOT DISTURBED OR OBLITERATED BEFORE SURVEY GROUND CONTROL POINTS ARE LOCATED, VERIFIED, AND DEEMED ADEQUATE FOR CONSTRUCTION LAYOUT. THE CONSTRUCTION LAYOUT SHALL BE PROVIDED IN SUFFICIENT DETAIL, THEREBY ENABLING THE CONTRACTOR TO CONSTRUCT THE PROJECT IN CONFORMITY WITH THE PLANS AND SPECIFICATIONS. SURVEY WILL BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT BEGIN CONSTRUCTION ACTIVITIES UNTIL ADEQUATE SURVEY GROUND CONTROL POINTS HAVE BEEN ESTABLISHED, TIED DOWN, AND VERIFIED IN WRITING BY THE CONTRACTOR'S PROFESSIONAL LAND SURVEYOR.
- ALL R.I. STD. 9.9.0 CONSTRUCTION ACCESS ROADS SHALL BE CONSTRUCTED PRIOR TO ANY ROADWAY ACCEPTING CONSTRUCTION TRAFFIC.
- THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL ITEMS WILL BE DETERMINED BY THE CONTRACTOR TO MEET THE REQUIREMENTS OF SECTION 907.
- ALL SIDEWALK AND DRIVEWAYS DESIGNATED FOR REPLACEMENT SHALL BE CUT AND MATCHED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- ASPHALT EMULSION TACK COAT SHALL BE PLACED PRIOR TO PAVEMENT PLACEMENT ON THE CONCRETE BASE OR COLD PLANED PAVEMENT, AND ON ANY NEW COURSE WHICH HAS BEEN OPEN TO TRAFFIC, OR ANY NEW COURSE WHICH HAS BEEN EXPOSED FOR MORE THAN 7 DAYS, AND/OR AS DIRECTED BY THE ENGINEER. IT SHALL ALSO BE APPLIED TO VERTICAL PAVEMENT FACES BETWEEN ADJOINING PAVEMENT SECTIONS. ALL APPLICATIONS ON BOTH HORIZONTAL AND VERTICAL SURFACES SHALL BE INCIDENTAL TO THE APPLICABLE PAVEMENT ITEMS.
- THE LIMITS OF CLEARING AND SURFACE DISTURBANCE SHALL BE STRICTLY ADHERED TO IN ALL AREAS. IN ADDITION TO THOSE AREAS SPECIFICALLY DESIGNATED ON THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND PLACING, AT ITS OWN EXPENSE, PLANTABLE SOIL AND SEED IN AREAS WHICH ARE OUTSIDE OF THE PROJECT'S AREAS OF DISTURBANCE AND WHICH ARE IMPACTED BY CONSTRUCTION OPERATIONS INCLUDING THOSE AREAS WHERE VEHICLES, EQUIPMENT AND MATERIALS ARE STORED.
- THE CONTRACTOR WILL NOT BE ALLOWED TO STOCKPILE REMOVED PAVEMENT MATERIALS WITHIN THE PROJECT LIMITS.
- CLEANING AND SWEEPING OF PAVEMENT WILL INCLUDE REMOVAL OF ALL PAVEMENT DEBRIS PRIOR TO THE PLACEMENT OF EACH BITUMINOUS PAVEMENT LIFT. ALL CLEANING AND SWEEPING SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER. CLEANING WITH COMPRESSED AIR SHALL ONLY BE ALLOWED WITH THE APPROVAL OF THE ENGINEER.
- PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS AND SHOP DRAWINGS OR AS MODIFIED BY THE ENGINEER.
- THE COORDINATE SYSTEM, IF SHOWN, IS THE RHODE ISLAND STATE PLANE COORDINATE SYSTEM.
- PAVEMENT OPERATIONS FOR CURBED SECTIONS: IN AREAS WHERE CURBING IS SET TO FINISH LINE AND GRADE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE THE SENSOR AND SKY-TYPE DEVICE FOR AUTOMATIC GRADE CONTROL, BUT WILL BE ALLOWED TO MANUALLY ADJUST THE BITUMINOUS PAVER FOR CONTROLLING GRADE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ROADWAYS FREE OF DEBRIS RESULTING FROM THEIR CONSTRUCTION OPERATIONS. ALL DEBRIS SHALL BE REMOVED TO MAINTAIN THE SAFE TRAVEL OF THE PUBLIC AT NO ADDITIONAL COST TO THE STATE.
- NO FUEL STORAGE, VEHICLE REFUELING, OR EQUIPMENT STORAGE SHALL TAKE PLACE IN DESIGNATED WETLANDS, NOR WITHIN 100' OF ANY WATER BODY. THIS REQUIREMENT SHALL NOT SUPERSEDE ANY FEDERAL, STATE OR LOCAL LAW, ORDINANCE, RULE OR REGULATION THAT APPLIES TO THE SAME, UNLESS THIS REQUIREMENT IS MORE STRINGENT THAN SAID LAW, ORDINANCE, RULE OR REGULATION.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT AT THE END OF FINAL PAVING OPERATIONS, FLOW TO NEW AND EXISTING DRAINAGE STRUCTURES HAS BEEN PROPERLY ESTABLISHED AND THAT NO ISOLATED DEPRESSIONS REMAIN. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; ANY CORRECTIVE ACTION SHALL BE CONSIDERED INCIDENTAL TO PAVING AND COLD PLANING OPERATIONS.
- ALL EMBANKMENTS SHALL BE PLACED IN HORIZONTAL LAYERS NOT EXCEEDING 12" (AFTER COMPACTION) AND SHALL BE COMPACTED AS SPECIFIED BEFORE THE NEXT LAYER IS PLACED. ALSO, EMBANKMENT CONSTRUCTION SHALL CONFORM TO SECTION 202.03.2 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- IF THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE, AS DESIGNATED ON THE COVERSHEET, THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAY SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.
- THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS STATED IN THE ENVIRONMENTAL APPROVALS ISSUED FOR THE PROJECT FROM THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM), AND/OR THE ARMY CORPS OF ENGINEERS (ACOE), AND/OR THE COASTAL RESOURCES MANAGEMENT COUNCIL (ORMC). COPIES OF EACH OF THESE PERMITS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH THESE CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).

GENERAL NOTES (CONTINUED):

- FOR ALL PROJECTS INVOLVING KNOWN SITE REMEDIATION ISSUES, THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE CONSTRUCTION RELATED PROVISIONS, CONDITIONS, AND STIPULATIONS OF ANY REMEDIAL ACTION WORK AND/OR SOIL MANAGEMENT PLANS DEVELOPED FOR THE PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH COMPLIANCE WITH THESE DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
- NO UNPROTECTED CONSTRUCTED FEATURE MAY PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. HEADWALL, DRAINAGE INLET, ETC.
- THE REMAINING SECTION OR STUB OF A BREAKAWAY BASE MAY NOT PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. SIGN POSTS, LIGHT POLES, FIRE HYDRANTS, ETC.

DRAINAGE AND EROSION CONTROL NOTES:

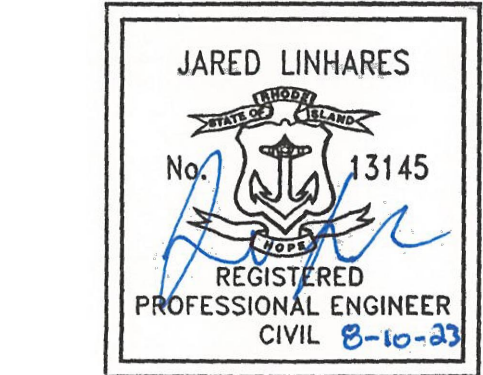
- THE CONTRACTOR IS REQUIRED TO ADHERE WITH THE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIPDES GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE-SPECIFIC SWPPP FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MODIFYING THE SWPPP AS SITE CONDITIONS WARRANT. A COPY OF THE SWPPP MUST BE ON-SITE AT ALL TIMES. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS.
- NO UNDISTURBED AREAS SHALL BE GRUBBED OF EXISTING VEGETATION AFTER OCTOBER 15 OF ANY CALENDAR YEAR OR DURING ANY PERIOD OF FULL OR LIMITED WINTER SHUTDOWN. ALL DISTURBED SOILS EXPOSED PRIOR TO OCTOBER 15 OF ANY CALENDAR YEAR SHALL BE SEEDED OR PROTECTED BY THAT DATE. ANY SUCH AREAS THAT DO NOT HAVE ADEQUATE VEGETATIVE STABILIZATION, AS DETERMINED BY THE RESIDENT ENGINEER OR ENVIRONMENTAL INSPECTOR, BY NOVEMBER 15 OF ANY CALENDAR YEAR, MUST BE STABILIZED THROUGH THE USE OF EROSION CONTROL MATTING OR HAY MULCH, IN ACCORDANCE WITH SPECIFICATIONS CONTAINED WITHIN THE R.I. SOIL EROSION AND SEDIMENT CONTROL HANDBOOK. IF WORK CONTINUES WITHIN ANY OF THESE AREAS DURING THE PERIOD FROM OCTOBER 15 THROUGH APRIL 15, CARE MUST BE TAKEN TO ENSURE THAT ONLY THE AREA REQUIRED FOR THAT DAY'S WORK IS EXPOSED, AND ALL ERODIBLE SOIL MUST BE RESTABILIZED WITHIN 5 WORKING DAYS. ANY WORK TO CORRECT PROBLEMS RESULTING FROM FAILURE TO COMPLY WITH THIS PROVISION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THERE WILL BE NO SEPARATE PAYMENT FOR THIS PROVISION, IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN 2 WEEKS OF FINAL GRADING.
- STOCKPILES OF MATERIAL SHALL NOT BE LOCATED WITHIN REGULATED WETLANDS OR BUFFER ZONE AREAS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND STOCKPILES OF ERODIBLE MATERIAL SHALL ALSO BE SEEDED AND RINGED WITH APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES TO STABILIZE. STOCKPILES OF CONTAMINATED MATERIALS MUST BE PLACED ON TOP OF A POLY-ETHYLENE SHEET AND COVERED AT ALL TIMES UNLESS IT IS AN ACTIVE WORKING PILE.
- IF THE PLANS INCLUDE SPECIFIC AREAS FOR PLACEMENT OF CONSTRUCTION Dewatering BASINS AND/OR EQUIPMENT AND MATERIALS STORAGE AND STOCKPILING, AND IF THE CONTRACTOR ELECTS TO UTILIZE ANY OTHER AREAS FOR THESE PURPOSES, THIS SHALL BE APPROVED BY THE ENGINEER ONLY AFTER OBTAINING ANY NECESSARY PERMITS AND/OR PERMIT MODIFICATIONS FROM THE APPROPRIATE REGULATORY AUTHORITY(IES). ANY PERMITTING REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE ACCOMPLISHED AT NO COST TO THE STATE. THE ENGINEER WILL COORDINATE SUBMISSION OF ANY REQUIRED PERMIT APPLICATION MATERIALS WITH THE R.I.D.O.T. ENVIRONMENTAL DIVISION.
- SURFACE EROSION CONTROL MATTING SHALL BE USED TO STABILIZE PLANTABLE SOIL AND/OR LOAM IN ALL DITCHES, ON ALL SLOPES ADJACENT TO WETLANDS AND WETLAND PERIMETERS, AND ON ALL SLOPES WITHIN WATER QUALITY BASINS. JUTE MESH IN DITCHES SHALL EXTEND TO AN ELEVATION 2 FEET ABOVE THE BOTTOM OF THE DITCH.
- SEEDING ON ALL SLOPES 3 TO 1 OR STEEPER SHALL CONSIST OF THE FOLLOWING APPLICATIONS UNLESS CHANGED IN THE CONTRACT.
 - SEEDING TYPE I.
 - ADHESIVE MULCH STABILIZER
- UNVEGETATED SLOPES SHALL NOT BE UNATTENDED OR EXPOSED FOR PERIODS IN EXCESS OF 2 WEEKS OR THROUGH THE INACTIVE WINTER SEASON.
- PRIOR TO CONSTRUCTION OPERATIONS, THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL CATCH BASINS AND FLUSHING THE PIPES, AND THEN VERIFYING THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING PIPES AND/OR STRUCTURES WHICH ARE TO BE CONNECTED. ANY VARIATION FOUND FROM THE PLANS MUST BE BROUGHT TO THE ENGINEER'S ATTENTION.
- ALL DRAINAGE AND UTILITY STRUCTURES WITHIN THE PAVED ROADWAY SHALL BE ADJUSTED TO GRADE WITH THE SURROUNDING PAVEMENT PRIOR TO THE WINTER SHUTDOWN.
- DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE EFFICACY OF THE DRAINAGE SYSTEM. ONCE CONSTRUCTION IS COMPLETED THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL CATCH BASINS AND FLUSHING ALL PIPES OF ANY CONSTRUCTION RELATED DEBRIS AT NO ADDITIONAL COST.
- CATCH BASIN RIM GRADES FOR STRUCTURES NOT IN A TRAVEL LANE NOTED ON PLANS ARE DEPRESSED 0.1' LOWER THAN THE GUTTER GRADE. RIM ELEVATIONS SHOWN ARE FINAL GRADES. THE CONTRACTOR SHALL PLACE FRAMES AND GRATES 0.1' BELOW THE GRADE CONSTRUCTED IN THIS CONTRACT OR AS DIRECTED BY THE ENGINEER.
- PROVISIONS FOR CLEARING TO ACCESS OUTFALLS DURING THE CLEANING AND FLUSHING OF THE CLOSED DRAINAGE SYSTEM SHALL STRICTLY ADHERE TO THE PLANS AND SPECIFICATIONS.
- THE CONTRACTOR SHALL INSTALL ALL SEDIMENT AND EROSION CONTROL DEVICES FOR OUTLET PROTECTION PRIOR TO CLEANING AND FLUSHING STORM WATER DRAINAGE. SEDIMENT AND EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ALL FLUSHED SEDIMENTS ARE REMOVED. AT ALL OUTFALL LOCATIONS WHERE PIPES ARE TO BE CLEANED AND FLUSHED, OUTLET PROTECTION (R.I. STD. 9.1.0 OR 9.3.0) SHALL BE INSTALLED TO TRAP SEDIMENTS. THESE SEDIMENTS SHALL THEN BE REMOVED AND DISPOSED OF LEGALLY BEFORE THE OUTLET PROTECTION DEVICES ARE REMOVED. IF OUTLET PROTECTION AT THE OUTFALL IS NOT FEASIBLE, THEN THE OUTLET PIPE OF THE LAST DRAINAGE STRUCTURE TO BE CLEANED SHALL BE PLUGGED TO CAPTURE ALL MATERIALS FLUSHED FROM PIPES. AFTER THE MATERIALS ARE REMOVED FROM THE DRAINAGE STRUCTURE, THE OUTLET SHALL BE UNPLUGGED TO RESUME NORMAL FUNCTIONING.
- R.I. STD. 9.8.0 BALED STRAW INLET PROTECTION SHALL BE INSTALLED AT ALL CATCH BASINS AND INLETS WHENEVER SUBBASE IS EXPOSED, AND SHALL REMAIN IN PLACE UNTIL THE ABUTTING GROUND SURFACES ARE STABILIZED.
- WHERE BALED STRAW INLET PROTECTION AND SILT FENCES ARE USED AT CATCH BASINS, THEY SHALL BE REMOVED AT THE END OF THE PROJECT OR AS DIRECTED BY THE ENGINEER IN ORDER TO PREVENT CLOGGING OF THE INLET.

DRAINAGE AND EROSION CONTROL NOTES (CONTINUED):



- DETENTION AND RETENTION BASINS MAY BE ROUGH GRADED AND STABILIZED WITH VEGETATION AND/OR OTHER EROSION CONTROL MEASURES AS REQUIRED BY THE ENGINEER PRIOR TO USE AS TEMPORARY SEDIMENTATION BASINS DURING PROJECT CONSTRUCTION. FINAL BASIN CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL SOURCES OF SEDIMENT HAVE BEEN REMOVED AND INFILTRATION IS REESTABLISHED. FINAL ROADSIDE VEGETATION IS ESTABLISHED AND USE OF TEMPORARY BASINS IS NO LONGER REQUIRED TO COMPLY WITH THE PLANS, SPECIFICATIONS, AND PERMITS. ANY ISSUES RELATING TO EROSION AND/OR SEDIMENT TRANSPORT INTO WETLAND AREAS RESULTING FROM SUCH USE OF SEDIMENTATION BASINS DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY CORRECTIVE ACTION AND COSTS REQUIRED TO RESOLVE SUCH ISSUES IS THE RESPONSIBILITY OF THE CONTRACTOR.
- THE TOE OF ANY FILL SLOPE IS TO REMAIN AT LEAST 1' INSIDE OF ALL EROSION CONTROLS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR COVER ANY PORTION OF THE EROSION CONTROL MEASURES WITH MATERIAL. ANY MATERIAL THAT IS PLACED ON ANY EROSION CONTROLS BY THE CONTRACTOR, OR ANY AGENT OF THE CONTRACTOR, SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR, AND ANY NECESSARY REPAIRS TO THE EROSION CONTROLS ACCOMPLISHED.
- PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES, EROSION AND SEDIMENTATION CONTROLS SHALL BE INSTALLED AT THOSE AREAS INDICATED ON THE PLANS. CLEARING MAY OCCUR PRIOR TO INSTALLATION OF SUCH CONTROLS, HOWEVER NO GRUBBING, GRADING, FILLING, OR OTHER SOIL DISTURBANCE SHALL OCCUR PRIOR TO INSTALLATION. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS.
- ALL COMPOST FILTER SOCK, STRAW BALES, SILT FENCE OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS IS ESTABLISHED. IF NEEDED, TEMPORARY SEEDING CAN HELP TO MINIMIZE EROSION. TEMPORARY SEED WILL CONFORM TO R.I.D.O.T. STANDARD TEMPORARY SEED MIX.
- THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE YEAR AND SHALL DO SO AT NO ADDITIONAL EXPENSE TO THE STATE.
- THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE SPECIFIED IN SUBSECTION L.02.03 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEMS. ADDITIONAL SEDIMENT AND EROSION CONTROLS, SHALL BE INSTALLED IN ACCORDANCE WITH THE SWPPP REPORT. THESE ADDITIONAL ITEMS WILL BE PAID AT THE UNIT PRICE FOR THAT BID ITEM.
- ANY OBSERVATIONS OF ILLICIT CONNECTIONS OR DISCHARGES TO RIDOT'S DRAINAGE NETWORK OR OUTFALLS SHALL BE REPORTED TO THE RIDOT STORMWATER UNIT IMMEDIATELY.

UTILITY NOTES:

- EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS USING THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE. BUILDING SERVICE CONNECTIONS (ELECTRIC, GAS, TELEPHONE, WATER AND SANITARY) ARE NOT SHOWN. CONTRACTOR IS TO ASSUME SERVICES ARE PRESENT TO ALL BUILDINGS.
- THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING DRAINAGE AND UTILITIES BOTH UNDERGROUND AND OVERHEAD BEFORE EXCAVATION BEGINS IN ACCORDANCE WITH CHAPTER 39-1.2 OF THE R.I. GENERAL LAWS ENTITLED "EXCAVATION NEAR UNDERGROUND UTILITY FACILITIES", WITH AMENDMENTS EFFECTIVE AS OF NOVEMBER 1, 2009 AND, WHEN NECESSARY, BY CONTACTING THE INDIVIDUAL UTILITY COMPANIES. EXCAVATION SHALL BE IN ACCORDANCE WITH ALL STATUTES, ORDINANCES, RULES AND REGULATIONS OF ANY APPLICABLE CITY, TOWN, STATE OR FEDERAL AGENCY. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE DIG SAFE PROGRAM. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES AND AND ENSURE THAT ALL UTILITIES HAVE BEEN MARKED PRIOR TO COMMENCING THEIR WORK. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANY, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.
- ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE CAPPED.
- EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW WATER MAINS.
- UTILITY SERVICE CONNECTIONS SHALL BE MAINTAINED TO ALL EXISTING FACILITIES TO REMAIN.
- FIRE HYDRANTS SHALL NOT BE REMOVED FROM SERVICE WITHOUT WRITTEN AUTHORIZATION FROM THE FIRE DEPARTMENT OR THE WATER AUTHORITY.
- ALL NEW WATER LINES SHALL BE DISINFECTED TO THE SATISFACTION OF THE WATER AUTHORITY IN ACCORDANCE WITH THE SPECIFICATIONS.
- ALL UTILITY POLE RELATED WORK SHALL BE BY OTHERS.
- THE CONTRACTOR SHALL PROVIDE 72-HOUR ADVANCE NOTICE TO THE RIDOT TMC (401-222-2378) FOR WORK AROUND RIDOT OWNED INFRASTRUCTURE (DRAINAGE, LIGHTING, ITS EQUIPMENT, TOLL GANTRIES, COUNTING STATIONS, ETC.). ANY DAMAGE TO THIS INFRASTRUCTURE MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT RIDOT IN ADVANCE, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.



THIS PLAN SHALL NOT BE ALTERED

 www.BETA-Inc.com	 RHODE ISLAND DEPARTMENT OF TRANSPORTATION	DESIGNED BY: WAC CHECKED BY: JML/KMA DATE: 8/10/23 SHEET: 3 OF: 12	SCALE: NOT TO SCALE						POTTER STREET AT WARREN AVENUE PARKING LOT			
			REVISIONS			REVISIONS						
			NO.	DATE	BY	NO.	DATE	BY	EAST PROVIDENCE		RHODE ISLAND	
			1	4/07	TRB	4	12/22	JRP				
	2	3/10	RBH									
	3	4/14	MLP									
										STANDARD NOTES - 1		

1. ALL PLANT MATERIAL MUST BE TAGGED AT THE NURSERY (A RECOGNIZED GROWER OF PLANT MATERIAL) IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION. ALL PLANT MATERIAL MUST BE NURSERY GROWN; NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED.
2. ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
3. ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE R.I.D.O.T. LANDSCAPE ARCHITECTURE UNIT.
4. COORDINATE WITH THE R.I.D.O.T. CONSTRUCTION MANAGER PRIOR TO ALL TRIMMING AND CLEARING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
5. ANY TOPSOIL USED AS PLANTABLE SOIL SHALL HAVE A SANDY LOAM TEXTURE, RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS, AND SHALL CONFORM TO SECTION M.18 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
6. ALL TREES AND SHRUBS SHALL BE MULCHED WITH PINE BARK MULCH IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
7. ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE MULCHED AS A BED.
8. PROVIDE A MINIMUM 6'-8" BRANCHING STANDARD ON ALL TREES INSTALLED ADJACENT TO SIDEWALKS AND/OR PEDESTRIAN ACCESS AREAS.
9. THE CONTRACTOR SHALL PROVIDE CERTIFICATION THAT THERE ARE NO CONTAMINANTS THAT EXCEED THE R.I.D.E.M. PERMISSIBLE LEVELS IN THE SOILS USED AS LOAM OR PLANTABLE SOIL.

JARED LINHARES

STATE OF FLORIDA

No. 13145

REGISTERED

PROFESSIONAL ENGINEER

CIVIL

8-10-23

STANDARD NOTES - 2



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY: WAC
CHECKED BY: JML/KMA
DATE: 8/10/23
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OF: 12

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REVISIONS			REVISIONS		
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EAST PROVIDENCE

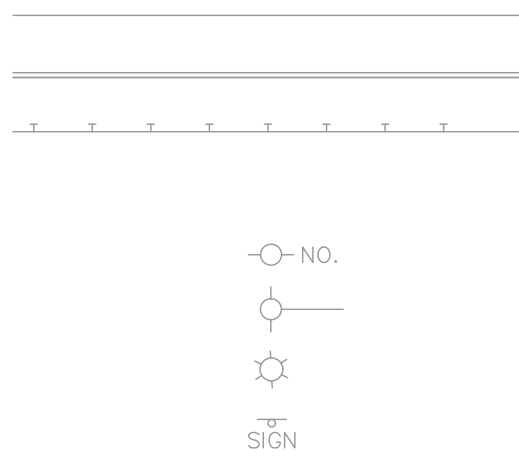
RHODE ISLAND

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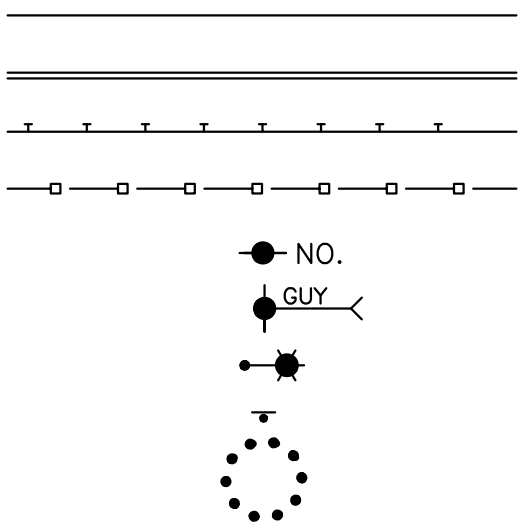
RI CONTRACT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
—	2024	5	12

JOB SPECIFIC PLAN SYMBOLS

EXISTING



NEW



JOB SPECIFIC LEGEND

CFS	COMPOST FILTER SOCK (SEE DETAIL ON SHT. 6)
DGC	REMOVAL AND DISPOSAL OF CURBING (ITEM 201.0436)
DT	CUTTING AND DISPOSING ISOLATED TREES AND STUMPS, ALL SIZES
ISCD	INLET SEDIMENT CONTROL DEVICE (SEE DETAIL ON SHT. 6)
RET	RETAIN
RRS	REMOVE AND RELOCATE DIRECTIONAL REGULATORY AND WARNING SIGN (ITEM T15.0200)
S-1	FULL DEPTH SAWCUT
4W	4" EPOXY RESIN PAVEMENT MARKINGS - WHITE
43.1.0 (m)	CEMENT CONCRETE SIDEWALK (MODIFIED)

ABBREVIATIONS

ABAN	ABANDON	RET	RETAINING
ADJ	ADJUST	R&R	REMOVE AND RESET
APPROX	APPROXIMATE	R&S	REMOVE AND STACK
℄	BASELINE	RT	RIGHT
BM	BENCH MARK	ROW	RIGHT-OF-WAY
BIT	BITUMINOUS	RD	ROAD
BB	BITUMINOUS BERM	SHT	SHEET
BC	BITUMINOUS CURB	SHLDR	SHOULDER
BOC	BOTTOM OF CURB	SDWK	SIDEWALK
BOS	BOTTOM OF SLOPE	SB	SOUTH BOUND OR STONE BOUND
BOW	BOTTOM OF WALL	SHL	STATE HIGHWAY LAYOUT LINE
BD OR BND	BOUND	STA	STATION
BLDG	BUILDING	SSD	STOPPING SIGHT DISTANCE
CEM	CEMENT	TAN	TANGENT
℄	CENTER LINE	T	TANGENT DISTANCE OF CURVE/TRUCK PERCENTAGE
CLF	CHAIN LINK FENCE	TEB	TEMPORARY EASEMENT BOUNDARY
CONC	CONCRETE	TEMP	TEMPORARY
CONT	CONTINUOUS	TOC	TOP OF CURB
CONST	CONSTRUCTION	TOS	TOP OF SLOPE
CO	COUNTY	TOW	TOP OF WALL
Δ	DELTA ANGLE (CENTRAL ANGLE OF HORIZ CURVE)	TP	TURNING POINT
DHV	DESIGN HOURLY VOLUME	TYP	TYPICAL
DWY	DRIVEWAY	VAR	VARIABLE
EB	EAST BOUND	VERT	VERTICAL
EP, EOP	EDGE OF PAVEMENT	VC	VERTICAL CURVE
EL	ELEVATION	WB	WEST BOUND
ETW	EDGE OF TRAVEL WAY	WCR	WHEELCHAIR RAMP
EXIST	EXISTING	WD	WOOD
FLDSTN	FIELDSTONE	CB	CATCH BASIN
FDN	FOUNDATION	CBCI	CATCH BASIN WITH CURB INLET
GAR	GARAGE	CIP	CAST IRON PIPE
GRAN	GRANITE	CL	CLASS (PIPE, CONCRETE, EXCAVATION, ETC)
GRAV	GRAVEL	COND	CONDUIT
GND	GROUND	CAP	CORRUGATED ALUMINUM PIPE
HORIZ	HORIZONTAL	CMP	CORRUGATED METAL PIPE
HMA	HOT MIX ASPHALT	CPP	CORRUGATED PLASTIC PIPE
HSE	HOUSE	CSP	CORRUGATED STEEL PIPE
IP	IRON PIPE	CULV	CULVERT
JCT	JUNCTION	CI	CURB INLET
LT	LEFT	CS	CURB STOP
L	LENGHT OF CURVE	DI	DUCTILE IRON PIPE
LP	LOW POINT	EL (OR ELEV)	ELEVATION
MB	MAIL BOX	FM	FORCE MAIN
MAX	MAXIMUM	F&C	FRAME AND COVER
MIN	MINIMUM	F&G	FRAME AND GRATE
NB	NORTH BOUND	GIP	GALVANIZED IRON PIPE
NTS	NOT TO SCALE	GG	GAS GATE
O.C.	ON CENTER	GI	GUTTER INLET
PVMT	PAVEMENT	HDWL	HEADWALL
PEB	PERMANENT EASEMENT BOUNDARY	HYD	HYDRANT
PS	PLANTABLE SOIL BORROW	INV	INVERT ELEVATION
PCC	POINT OF COMPOUND CURVATURE	LP	LIGHT POLE
PC	POINT OF CURVATURE	LPS	LOW PRESSURE SERVICE CONNECTION
PRC	POINT OF REVERSE CURVATURE	MH	MANHOLE
PI	POINT OF INTERSECTION	PVC	POLY-VINYL-CHLORIDE PIPE
PT	POINT OF TANGENCY	PWW	PAVED WATER WAY
PVC	POINT OF VERTICAL CURVATURE	R&D	REMOVE & DISPOSE
PVI	POINT OF VERTICAL INTERSECTION	RCP	REINFORCED CONCRETE PIPE (CLASS III UNLESS NOTED)
PVT	POINT OF VERTICAL TANGENCY	S	SANITARY SEWER OR SERVICE CONNECTION
PGL	PROFILE GRADE LINE	SMH	SEWER MANHOLE
PROJ	PROJECT	SD	SUBDRAIN
℄	PROPERTY LINE	TS	TRAFFIC SIGNAL
PROPOSED	PROPOSED	TSC	TRAFFIC SIGNAL CONDUIT
R	RADIUS OF CURVATURE	UP	UTILITY POLE
R&D	REMOVE & DISPOSE	VCP	VITRIFIED CLAY PIPE
REM	REMOVE	WG	WATER GATE
REMOD	REMODEL	WM	WATER METER / WATER MAIN



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

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DATE: 8/10/23
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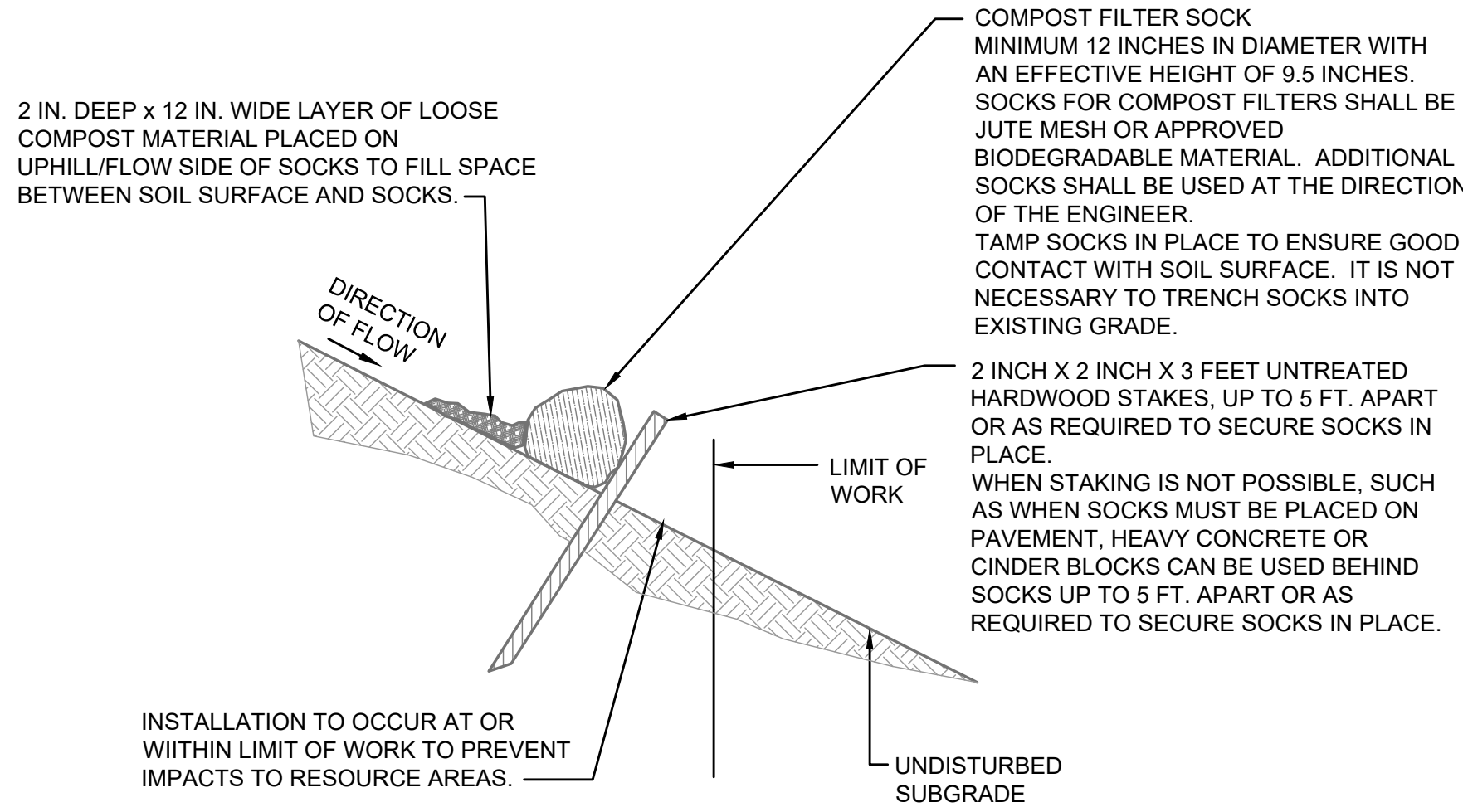
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REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

POTTER STREET AT
WARREN AVENUE PARKING LOT

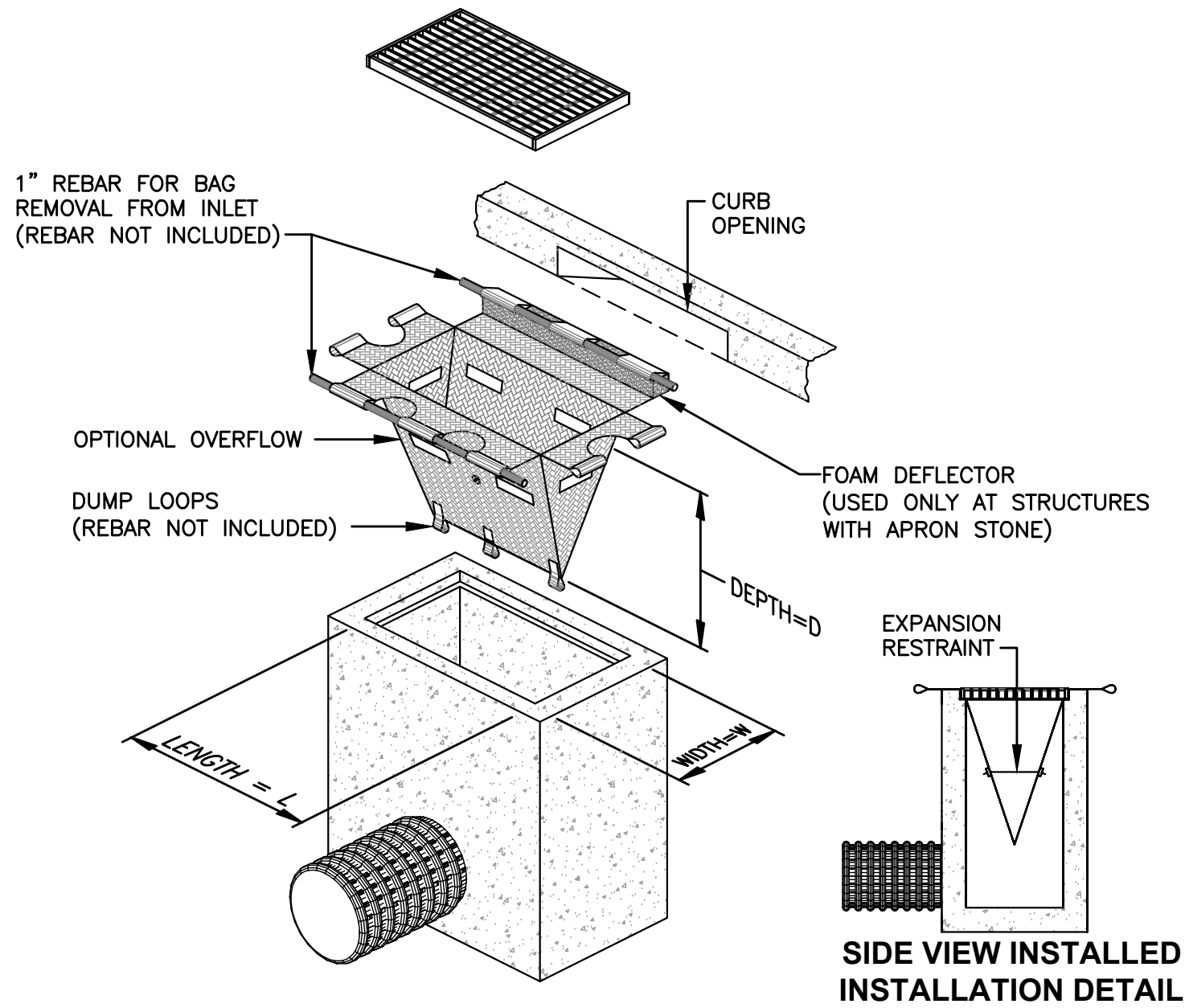
EAST PROVIDENCE
RHODE ISLAND

JOB SPECIFIC SYMBOLS LEGEND & NOTES

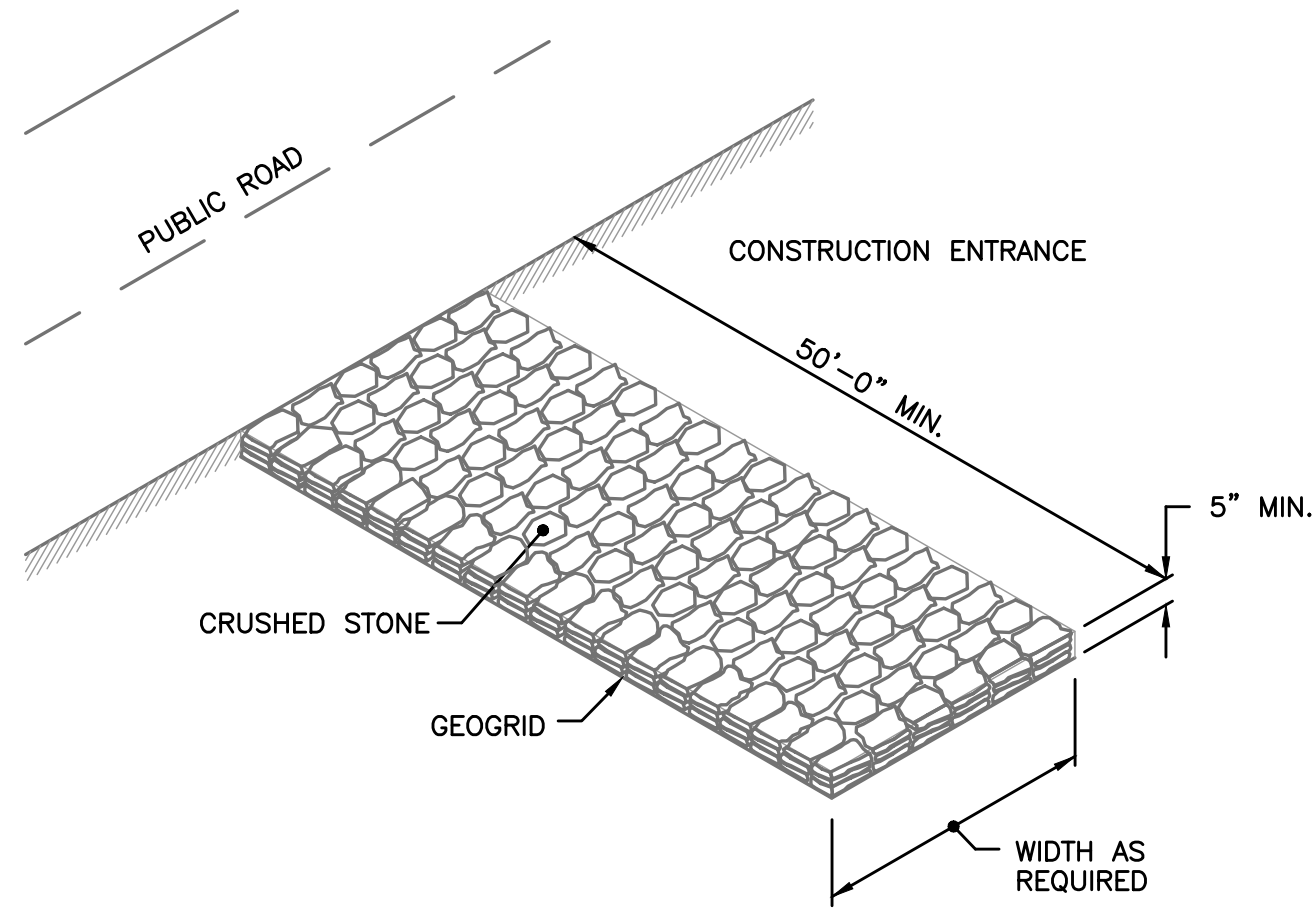


- NOTES:
1. PROVIDE A MINIMUM SOCK DIAMETER OF 12 INCHES FOR SLOPES UP TO 50 FEET IN LENGTH WITH A SLOPE RATIO OF 3H:1V OR STEEPER. LONGER SLOPES OF 3H:1V MAY REQUIRE LARGER SOCK DIAMETER OR ADDITIONAL COURSING OF FILTER SOCKS TO CREATE A FILTER BERM. REFER TO MANUFACTURER'S RECOMMENDATIONS FOR SITUATIONS WITH LONGER OR STEEPER SLOPES.
 2. INSTALL SOCKS ALONG CONTOURS AND PERPENDICULAR TO SHEET OR CONCENTRATED FLOW.
 3. DO NOT INSTALL IN PERENNIAL, EPHEMERAL OR INTERMITTENT STREAMS.
 4. CONFIGURE SOCKS AROUND EXISTING SITE FEATURES TO MINIMIZE SITE DISTURBANCE AND MAXIMIZE CAPTURE AREA OF STORMWATER RUN-OFF.

COMPOST FILTER SOCK (CFS)
NOT TO SCALE

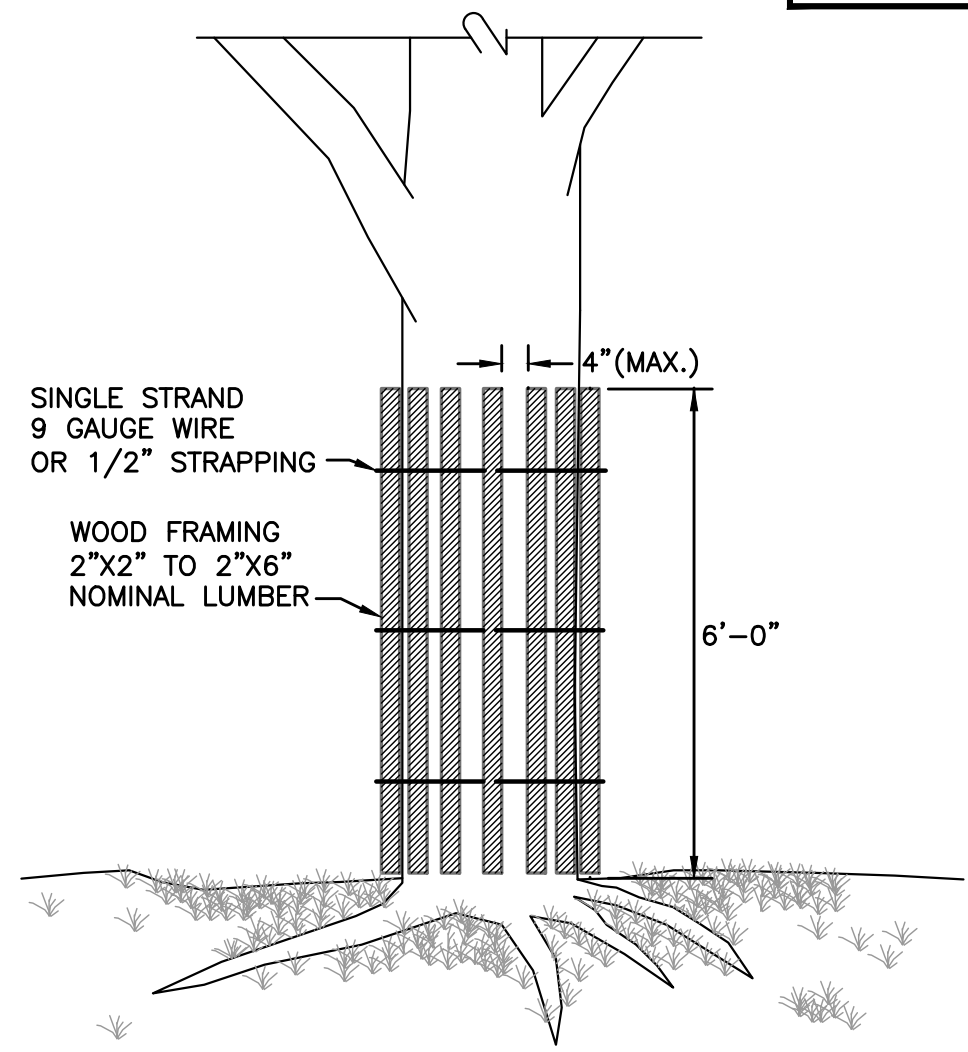


INLET SEDIMENT CONTROL DEVICE (WITH OPTIONAL CURB DEFLECTOR) (ISCD)
NOT TO SCALE



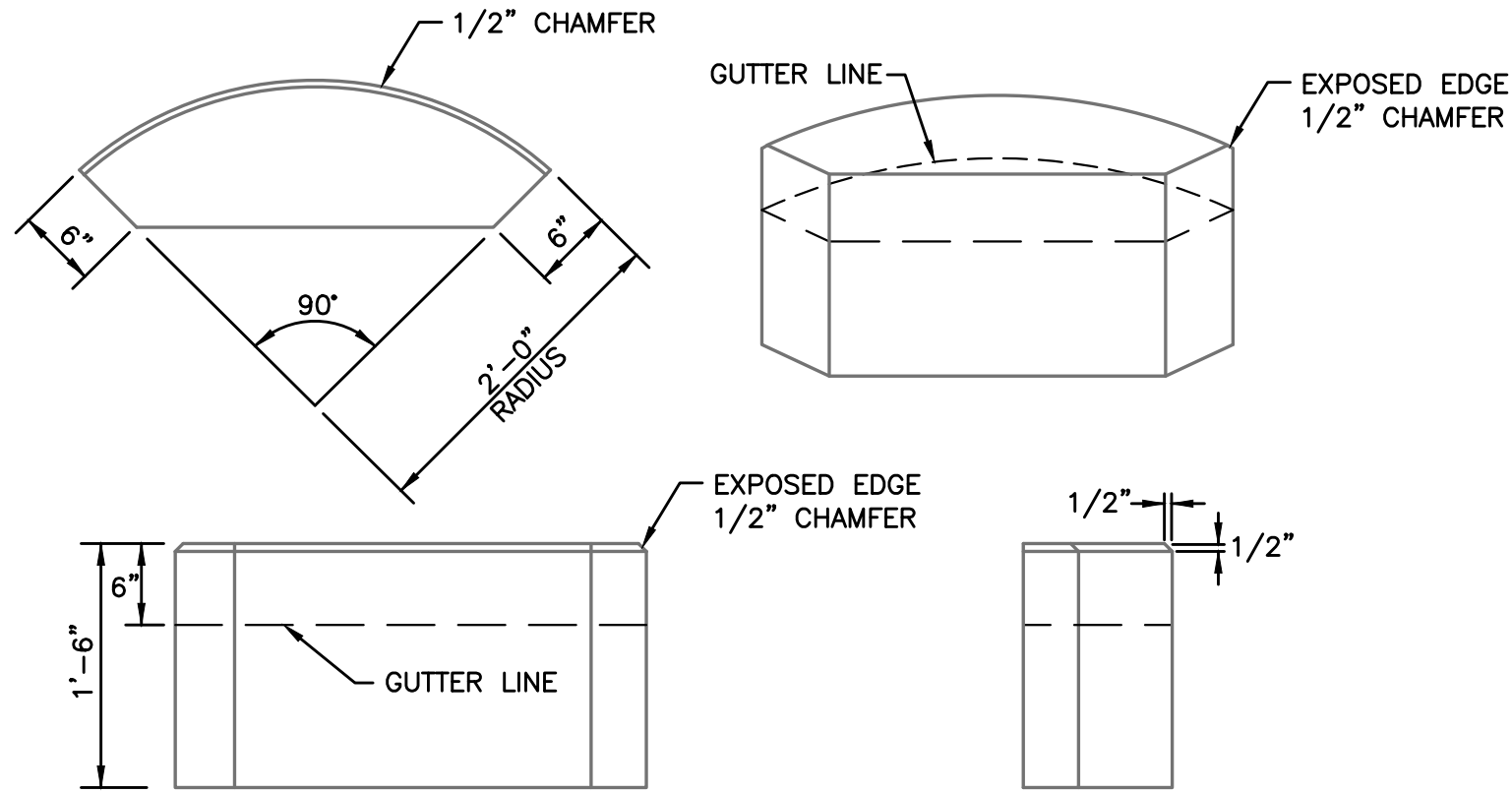
NOTE: SHALL BE IN ACCORDANCE WITH SECTION 211 OF THE R.I. STANDARD SPECIFICATIONS.

CONSTRUCTION ACCESS (R.I. STANDARD 9.9.0)
NOT TO SCALE



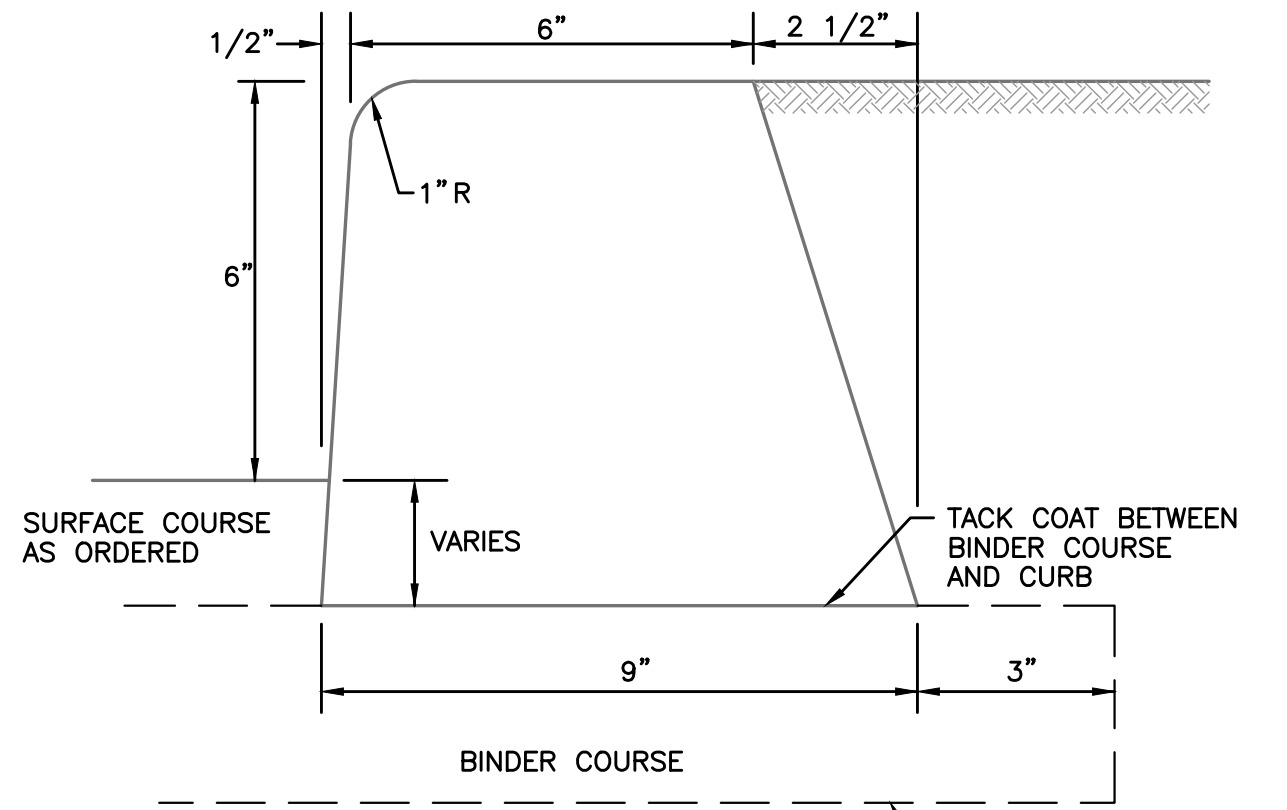
NOTE: SHALL BE IN ACCORDANCE WITH SECTION L.11 OF THE STANDARD SPECIFICATIONS.

TREE PROTECTION DEVICE (R.I. STANDARD 51.1.0)
NOT TO SCALE



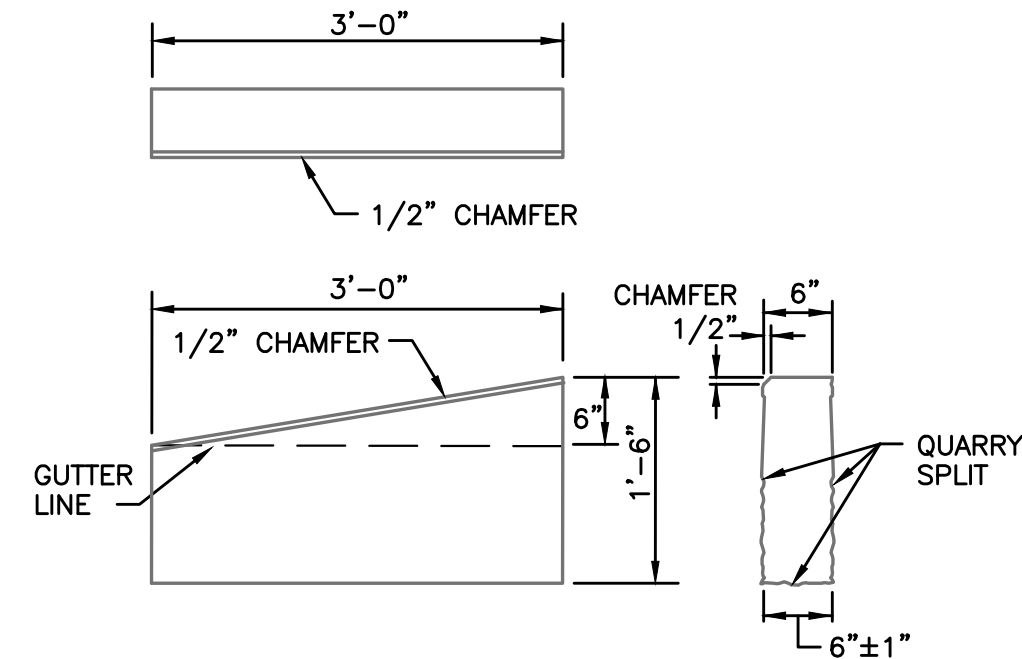
- NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS.
 2. TOP SURFACE TO BE DRESSED BY SAW. REMAINDER MAY BE QUARRY SPLIT.

GRANITE 2'-0\"/>



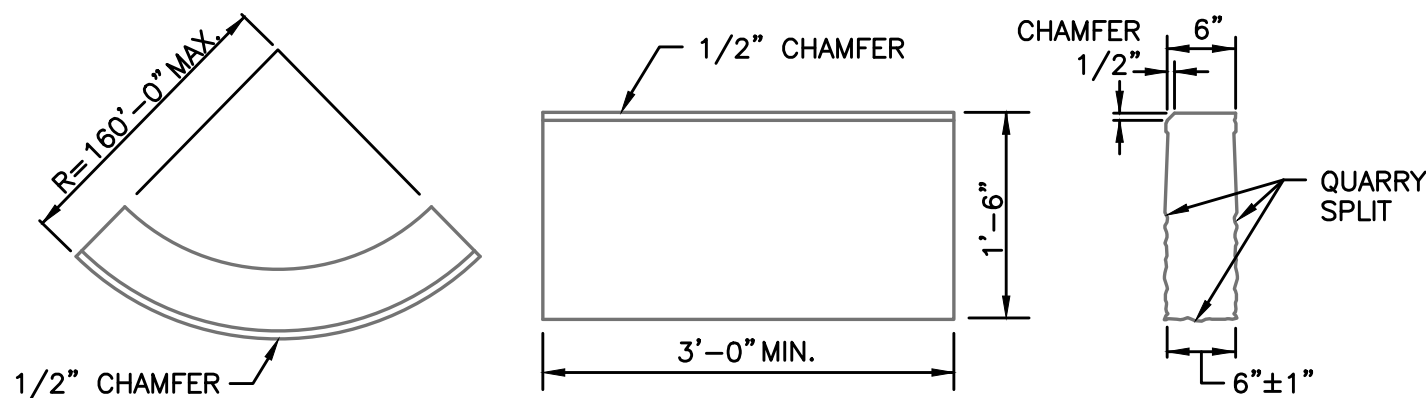
NOTE: SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS.

BITUMINOUS CONCRETE LIP CURB (R.I. STANDARD 7.5.0)
NOT TO SCALE



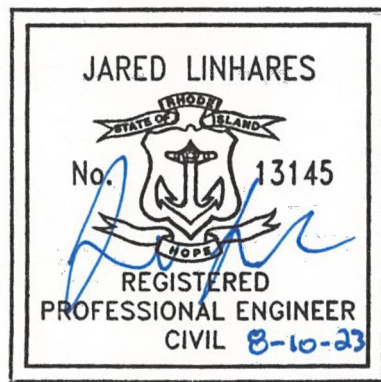
- NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS.
 2. TOP SURFACE TO BE DRESSED BY SAW. REMAINDER MAY BE QUARRY SPLIT.

3'-0\"/>



- NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS.
 2. TOP SURFACE TO BE DRESSED BY SAW. REMAINDER MAY BE QUARRY SPLIT.
 3. MINIMUM LENGTH OF STRAIGHT OR CIRCULAR PIECES TO BE 3'-0\"/>
 4. CIRCULAR CURB IS REQUIRED ON CURVES WITH RADII OF 160'-0\"/>

GRANITE CURB (R.I. STANDARD 7.3.0)
NOT TO SCALE



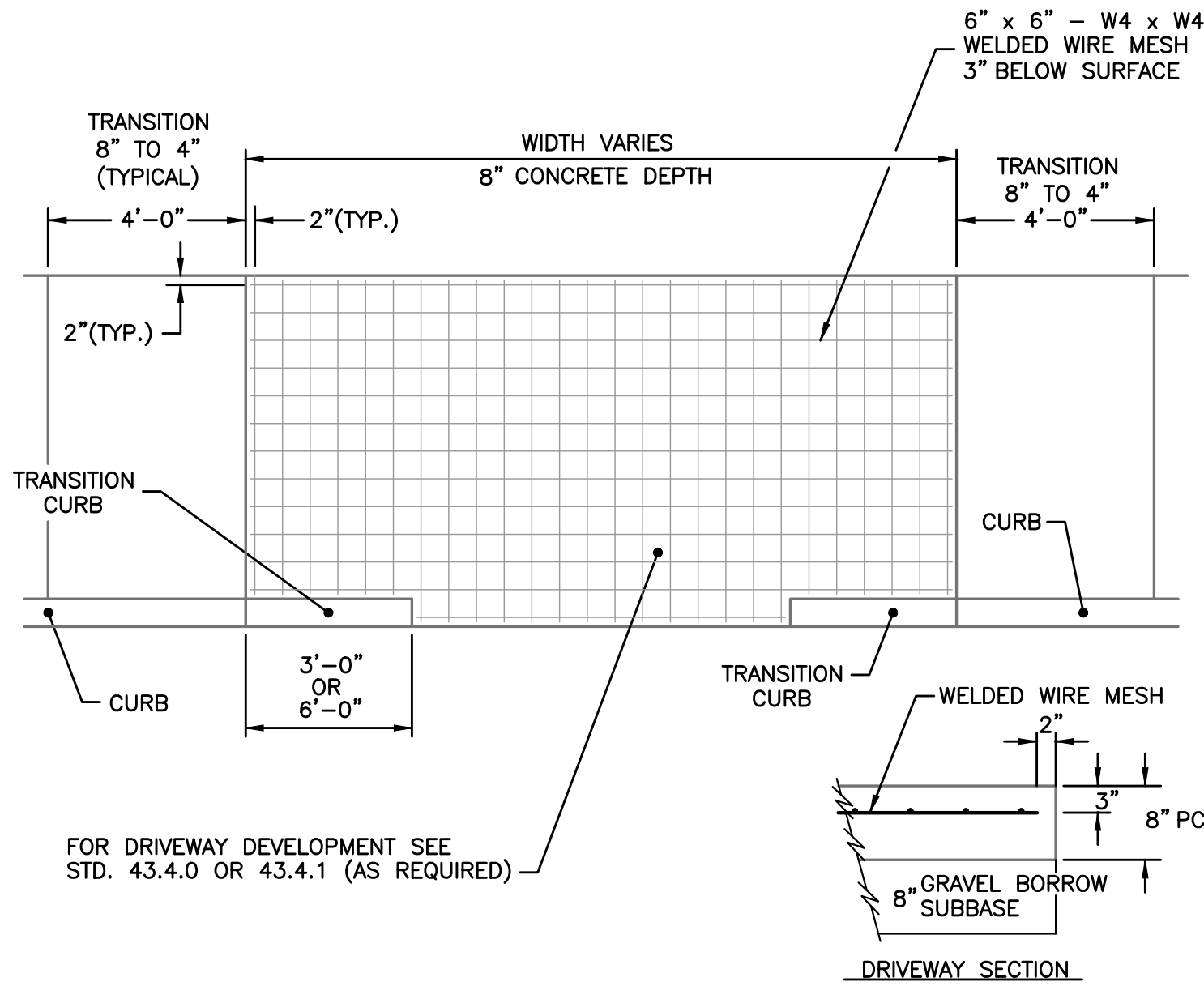
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY: WAC
CHECKED BY: JML/KMA
DATE: 8/10/23
SHEET: 6
OF: 12

SCALE: NOT TO SCALE

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NO.	DATE	BY	NO.	DATE	BY

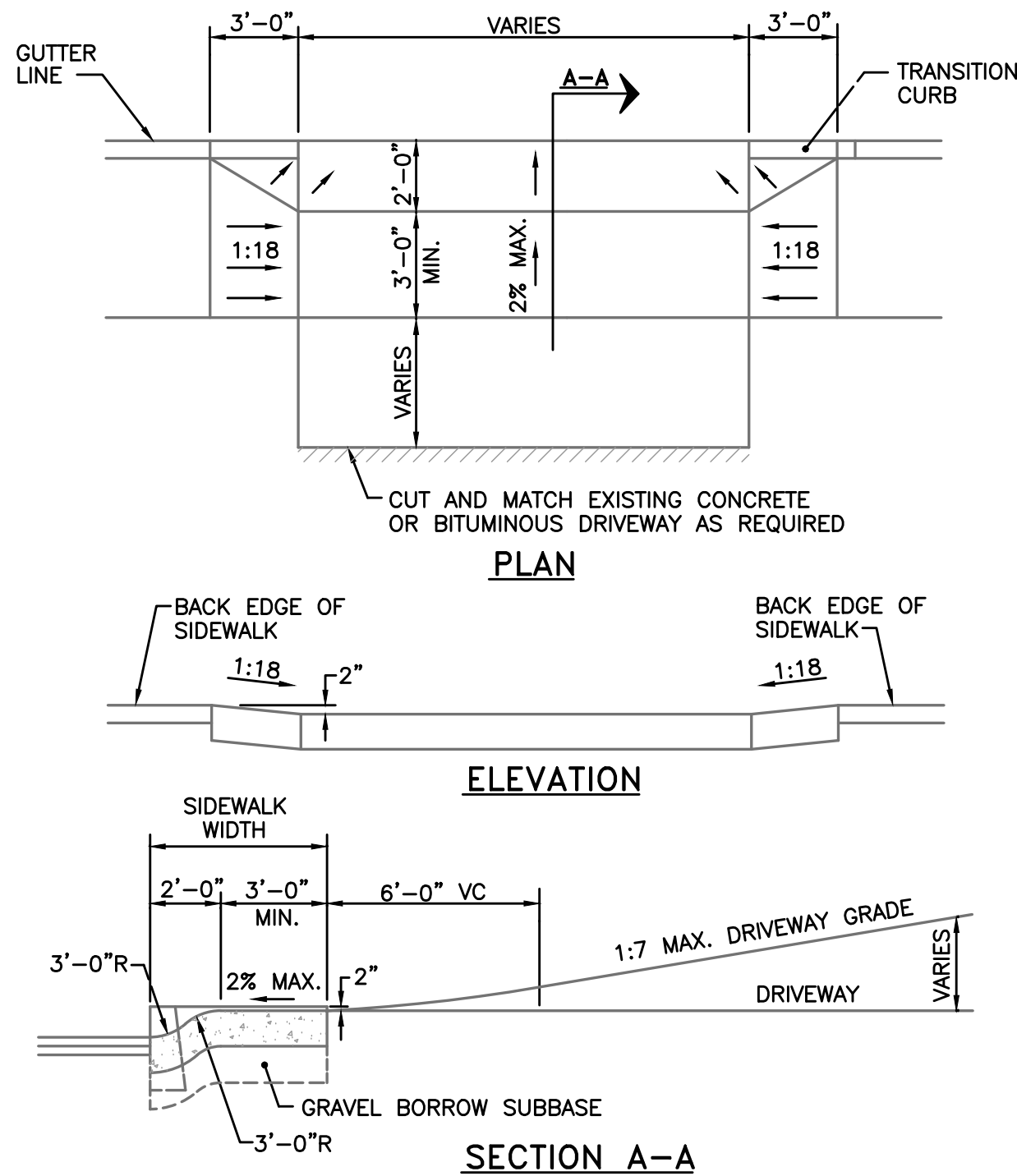
POTTER STREET AT
WARREN AVENUE PARKING LOT
EAST PROVIDENCE
RHODE ISLAND
CONSTRUCTION DETAILS-1



NOTE:
SHALL BE IN ACCORDANCE WITH SECTION 905 OF THE R.I. STANDARD SPECIFICATIONS.

CEMENT CONCRETE DRIVEWAYS
NOT TO SCALE

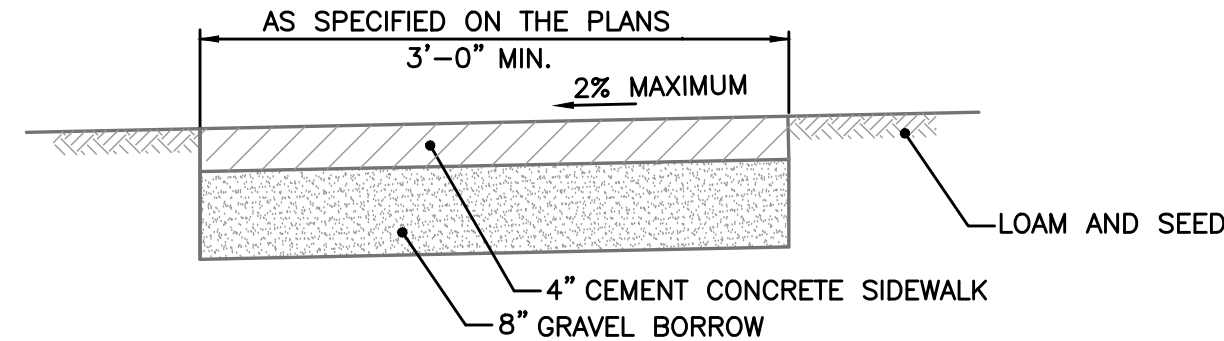
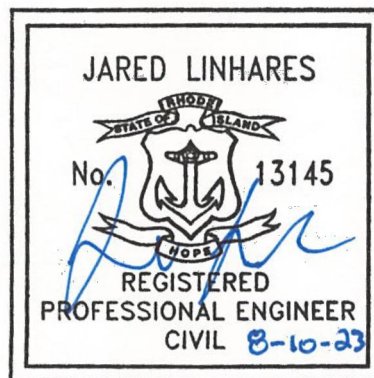
R.I. STANDARD
43.5.0



NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION 905 OF THE R.I. STANDARD SPECIFICATIONS.
2. WHEN DRIVEWAY IS BELOW BACK EDGE OF SIDEWALK PROFILE, STD. 43.4.1 MUST BE USED.

DRIVEWAY DEVELOPMENT FOR 3'-0" TRANSITION CURB
NOT TO SCALE

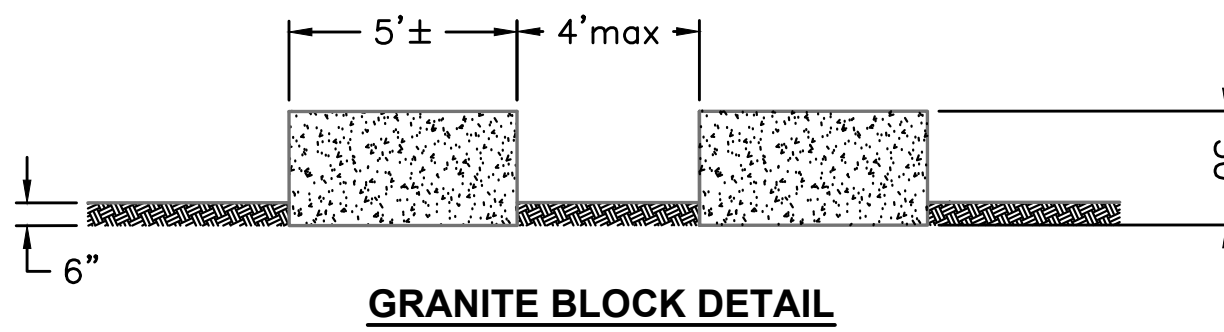
R.I. STANDARD
43.4.0



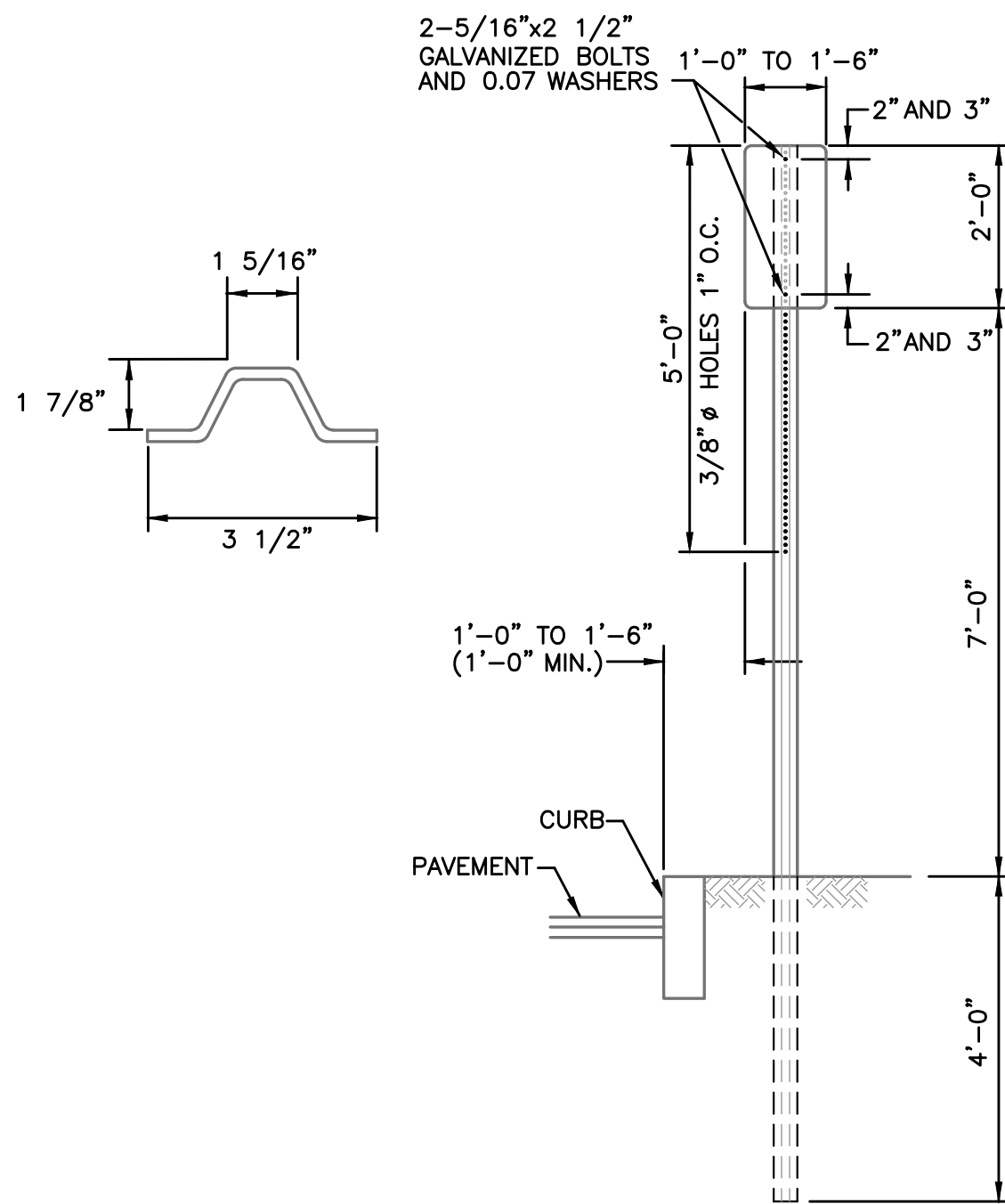
NOTES
1. SHALL BE IN ACCORDANCE WITH SECTION 905 OF THE R.I. STANDARD SPECIFICATIONS.

CEMENT CONCRETE SIDEWALK
NOT TO SCALE

R.I. STANDARD
43.1.0M



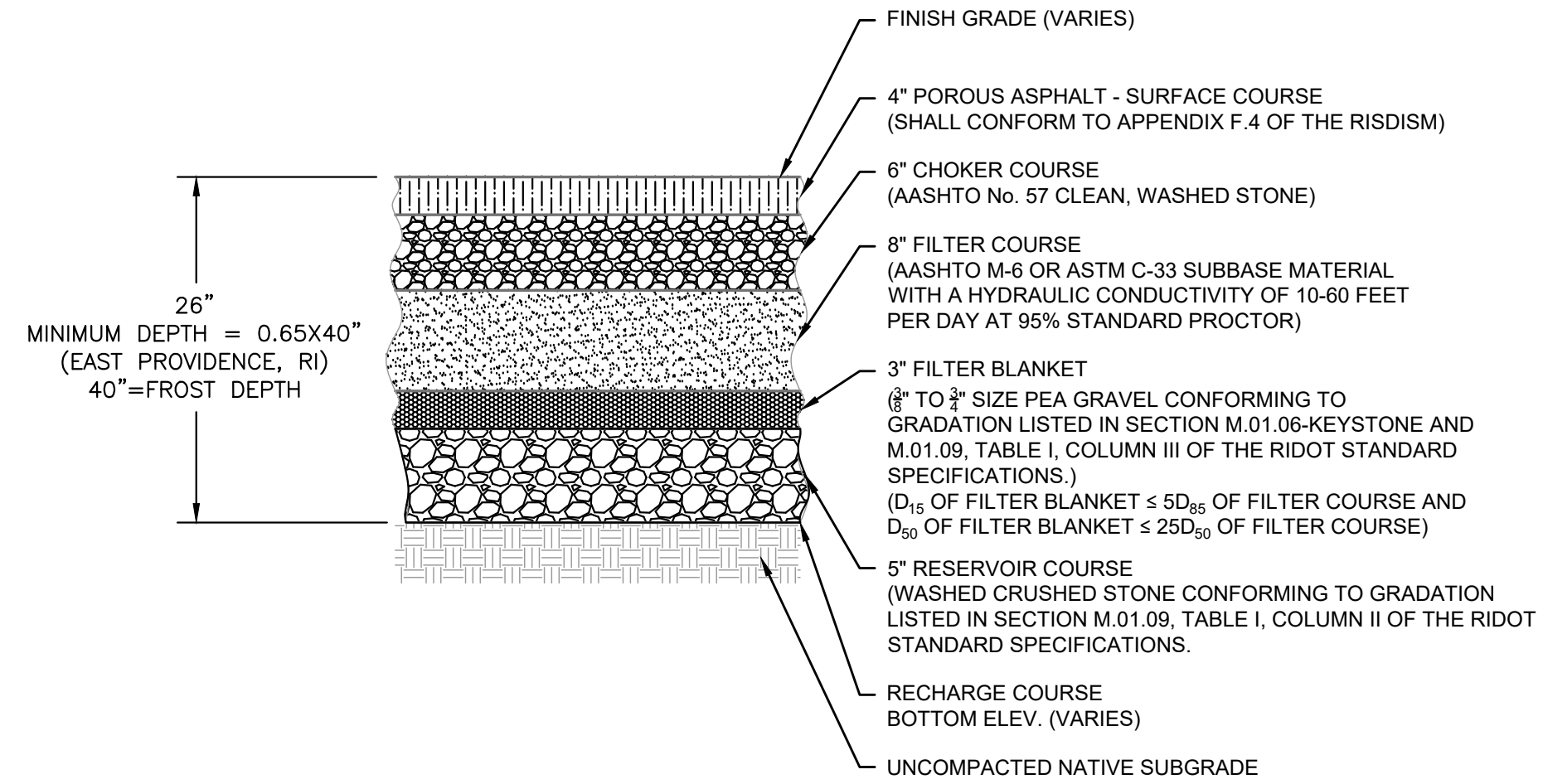
GRANITE BLOCK DETAIL



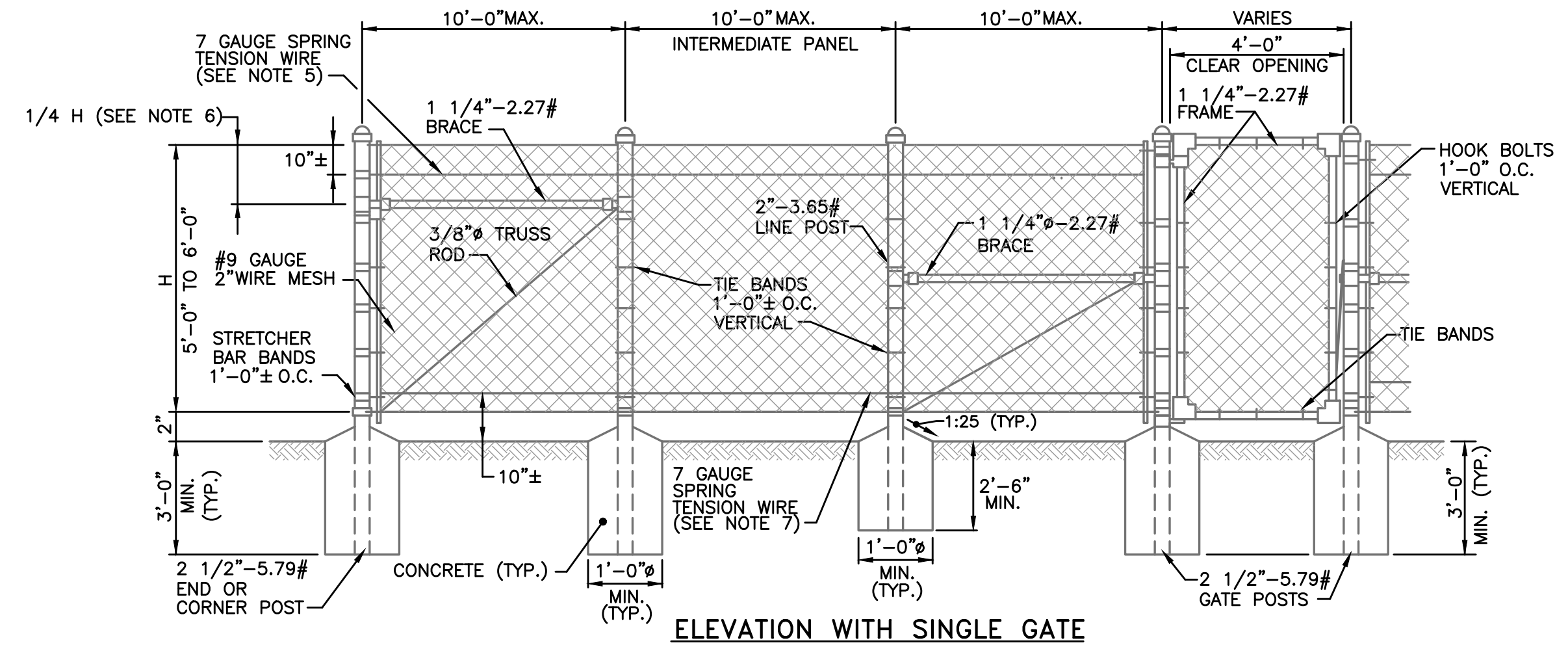
NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION T.15 OF THE STANDARD SPECIFICATIONS.
2. PARKING SIGNS SHALL BE SET AT AN ANGLE OF NOT LESS THAN 30° NOR MORE THAN 45° WITH A LINE PARALLEL TO FLOW OF TRAFFIC, 1'-6" (1'-0" MIN.) FROM EDGE OF CURB FACE.

PARKING SIGN MOUNTING DETAIL
NOT TO SCALE

R.I. STANDARD
24.6.0

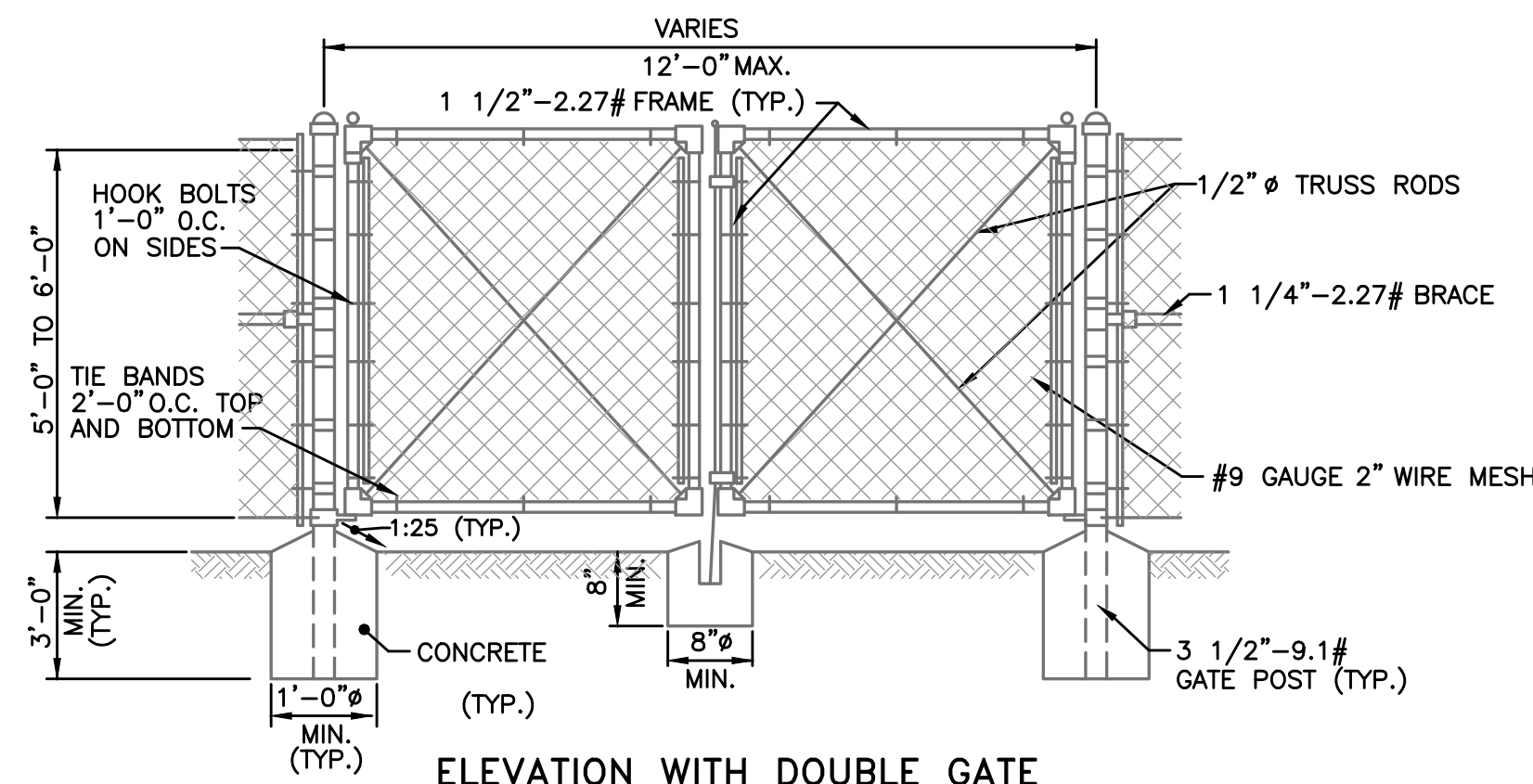


POROUS PAVEMENT SYSTEM
TYPICAL CROSS SECTION



ELEVATION WITH SINGLE GATE

NOTES:
1. SHALL BE IN ACCORDANCE WITH SECTION 903 OF THE R.I. STANDARD SPECIFICATIONS.
2. 6 GAUGE STREET CLIPS TO BE USED TO FASTEN SPRING TENSION WIRE TO LINE POST.
3. SPRING TENSION WIRE - NO. 7 GAUGE CORRUGATED HEAVILY GALVANIZED (2.0 OZ. PER SQ. FT.) ALUMINUM COATED (0.4 OZ. PER SQ. FT.)
4. ALL PIPES REFER TO SCHEDULE 40 NOMINAL PIPE SIZES.
5. A 1 1/4"-2.27# TOP RAIL MAY BE SUBSTITUTED FOR THE TOP TENSION WIRE WHEN THE FENCE IS NOT LOCATED IN THE CLEAR ZONE.
6. WHEN A TOP RAIL IS USED, LOCATE THE BRACE RAIL AT 1/2 H.
7. WHEN A TOP RAIL IS USED, DELETE THE BOTTOM SPRING TENSION WIRE.



ELEVATION WITH DOUBLE GATE

CHAIN LINK FENCE 5'-0" TO 6'-0"
NOT TO SCALE

R.I. STANDARD
31.2.0



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

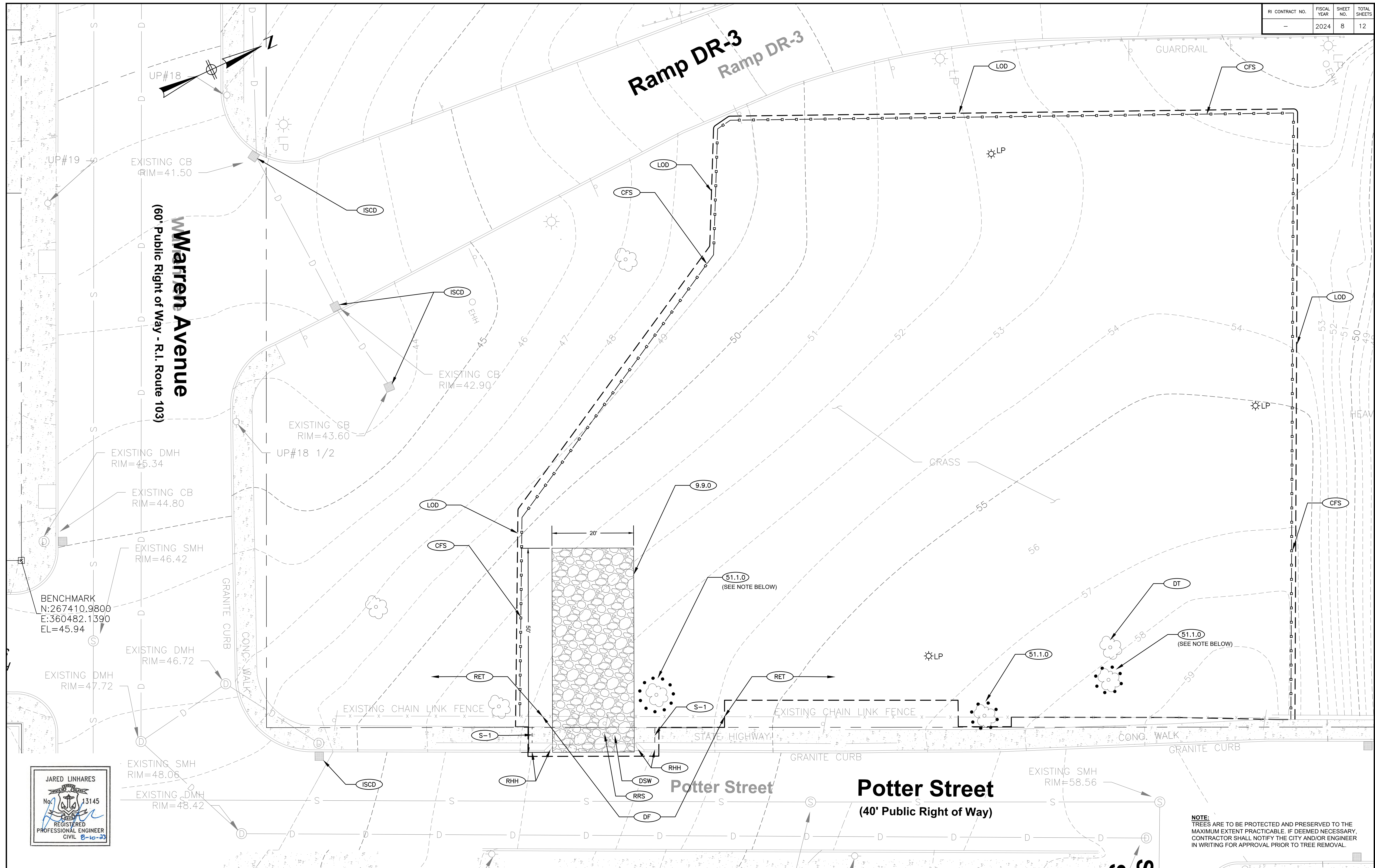
DESIGNED BY: WAC
CHECKED BY: JML/KMA
DATE: 8/10/23
SHEET: 7
OF: 12

SCALE: NOT TO SCALE




REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

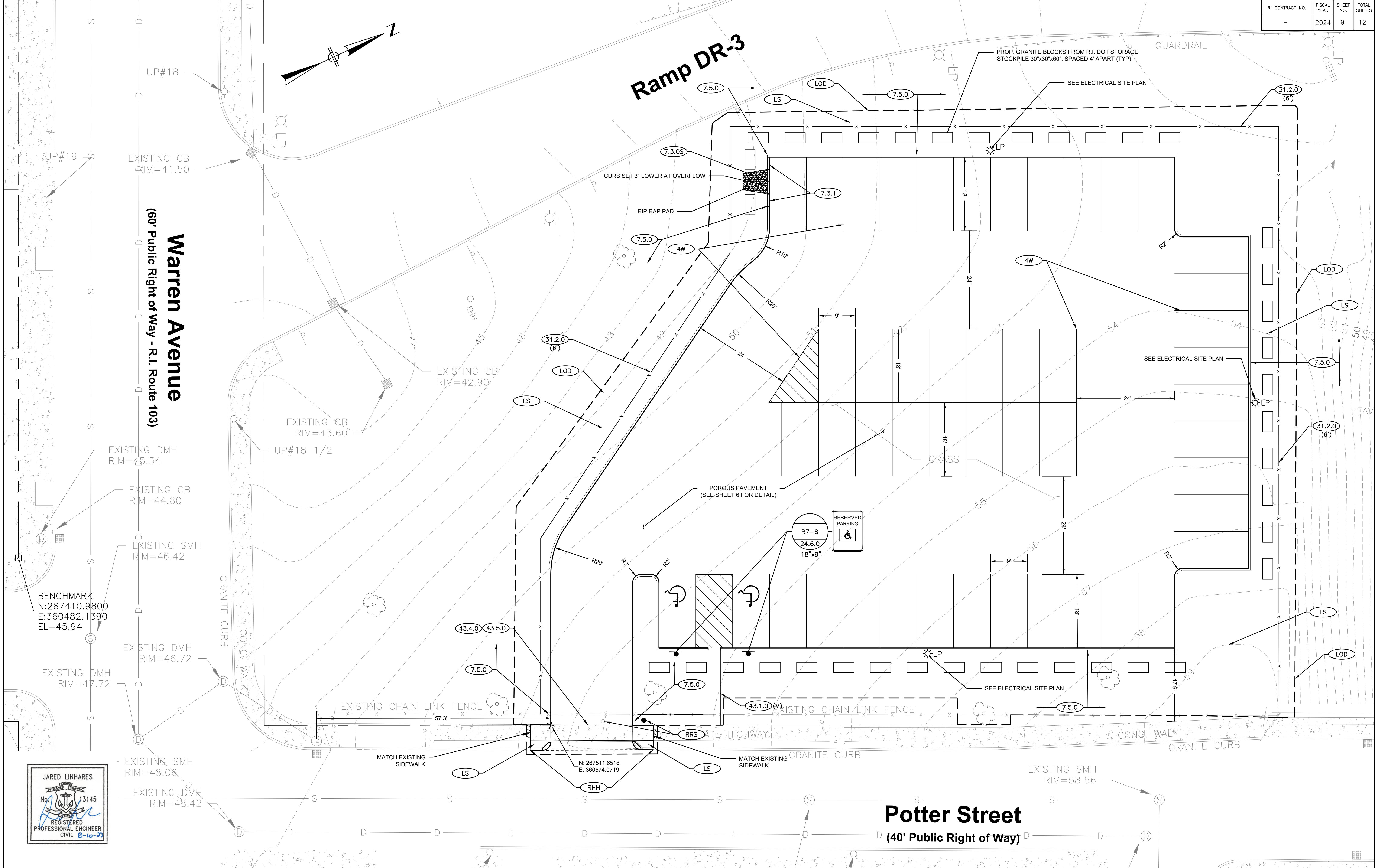
POTTER STREET AT
WARREN AVENUE PARKING LOT
EAST PROVIDENCE
RHODE ISLAND
CONSTRUCTION DETAILS-2

RI CONTRACT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
—	2024	8	12



NOTE:
TREES ARE TO BE PROTECTED AND PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE. IF DEEMED NECESSARY, CONTRACTOR SHALL NOTIFY THE CITY AND/OR ENGINEER IN WRITING FOR APPROVAL PRIOR TO TREE REMOVAL.

 www.BETA-Inc.com	 RHODE ISLAND DEPARTMENT OF TRANSPORTATION	DESIGNED BY: WAC CHECKED BY: JML/KMA DATE: 8/10/23 SHEET: 8 OF: 12	SCALE: 1"=10'  GRAPHIC SCALE	POTTER STREET AT WARREN AVENUE PARKING LOT
				EAST PROVIDENCE RHODE ISLAND
				SITE PREPARATION PLAN



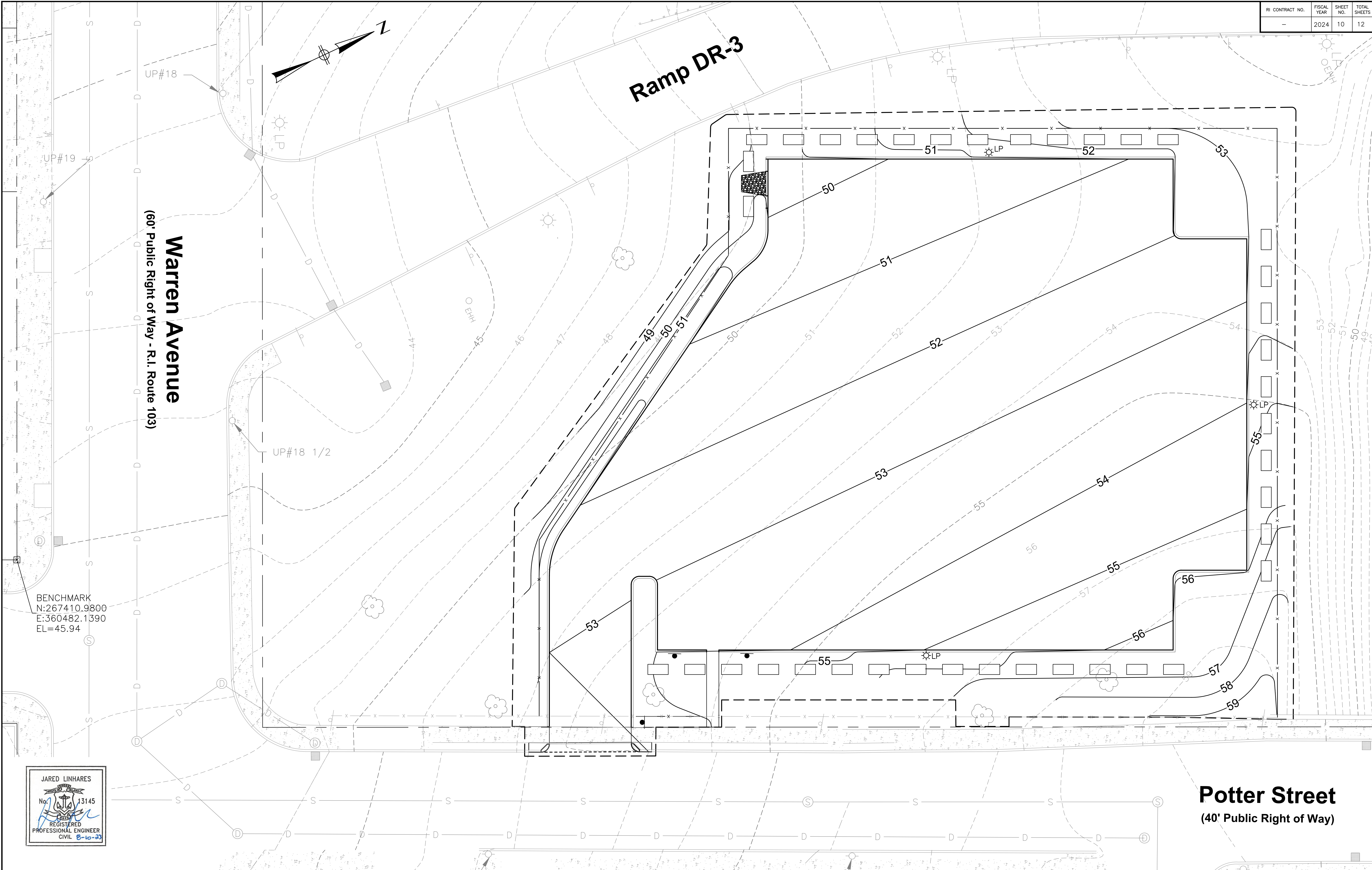
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY: WAC
CHECKED BY: JML/KMA
DATE: 8/10/23
SHEET: 9
OF: 12

SCALE: 1"=10'					
GRAPHIC SCALE					
REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

POTTER STREET AT
WARREN AVENUE PARKING LOT
EAST PROVIDENCE
RHODE ISLAND
CONSTRUCTION PLAN

FILE: N:\10357\10357 - POTTER STREET - EAST PROVIDENCE\DRAWINGFILES\PLANSET\10357_V1_012_GRA0 PLAN.DWG PLOT DATE: 8/11/2023 10:12 AM



RI CONTRACT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
-	2024	10	12

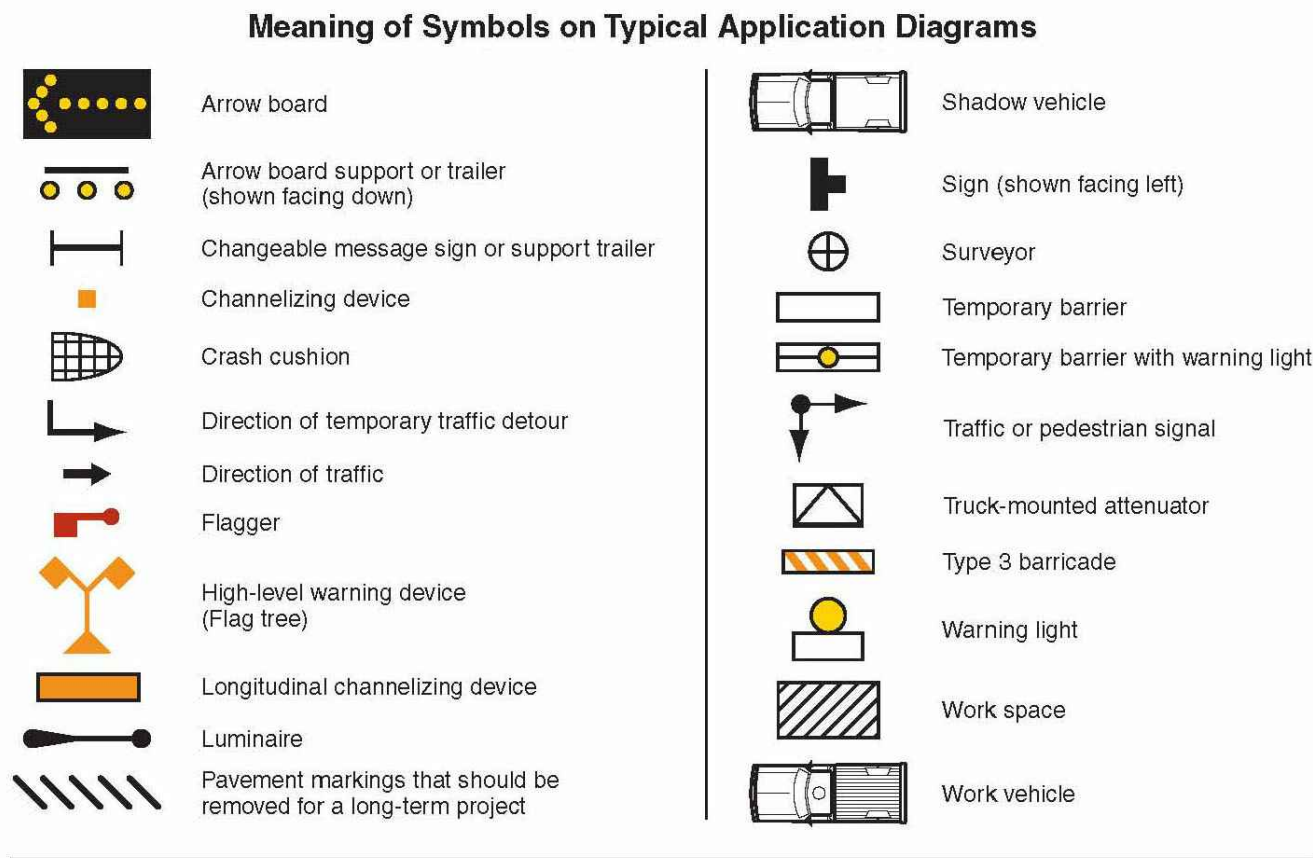


RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

DESIGNED BY: WAC
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DATE: 8/10/23
SHEET: 10
OF: 12

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NO.	DATE	BY	NO.	DATE	BY

POTTER STREET AT
WARREN AVENUE PARKING LOT
EAST PROVIDENCE
RHODE ISLAND
GRADING PLAN



Meaning of Letter Codes on Typical Application Diagrams

Road Type	Distance Between Signs**		
	A	B	C
Urban (low speed)*	100 feet	100 feet	100 feet
Urban (high speed)*	350 feet	350 feet	350 feet
Rural	500 feet	500 feet	500 feet
Expressway / Freeway	1,000 feet	1,500 feet	2,640 feet

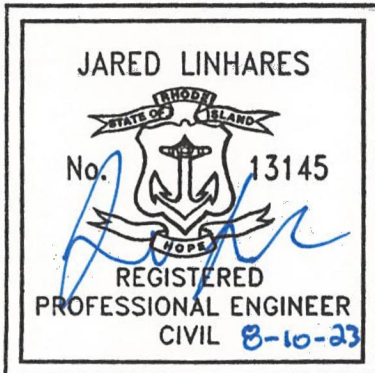
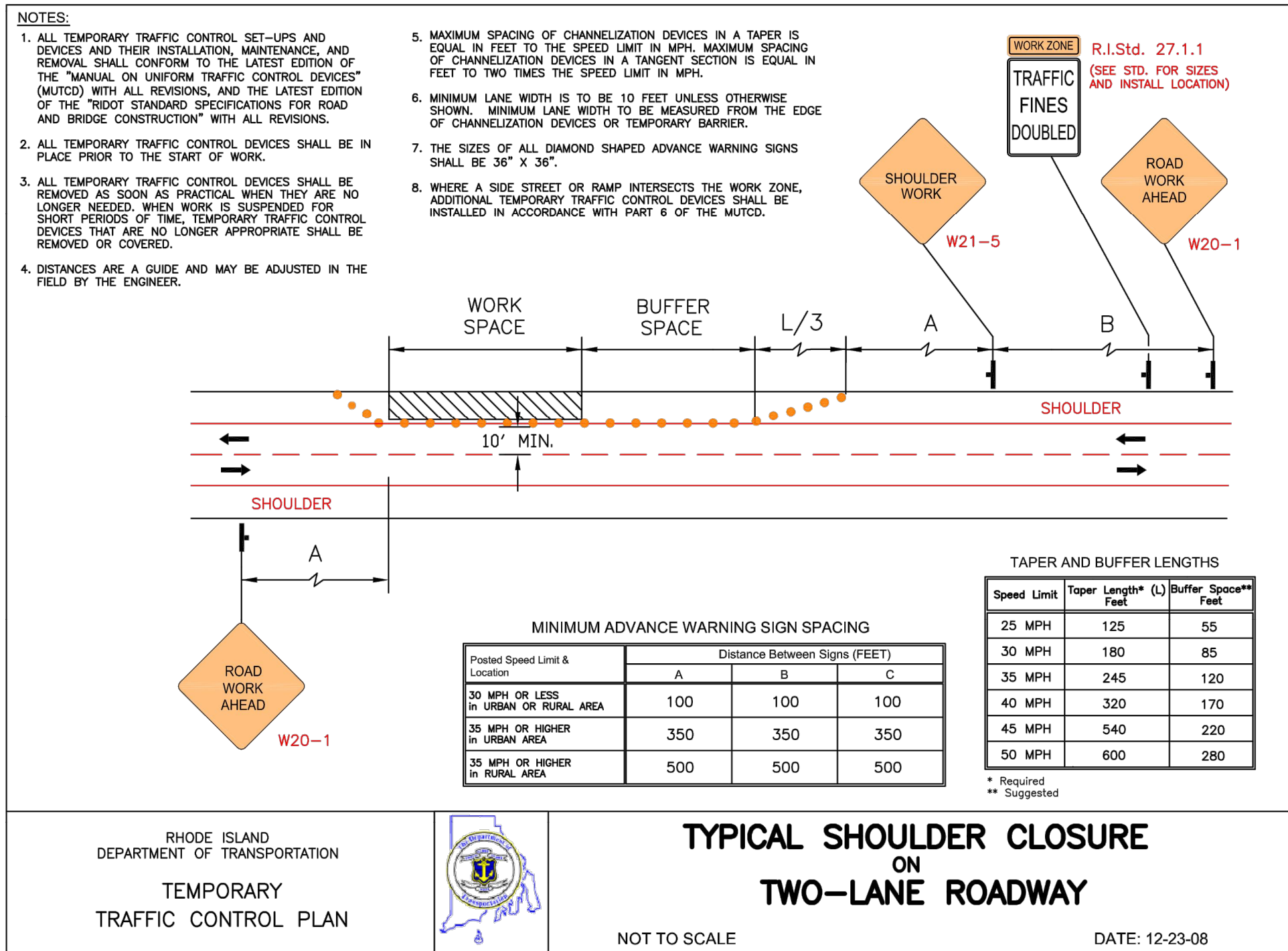
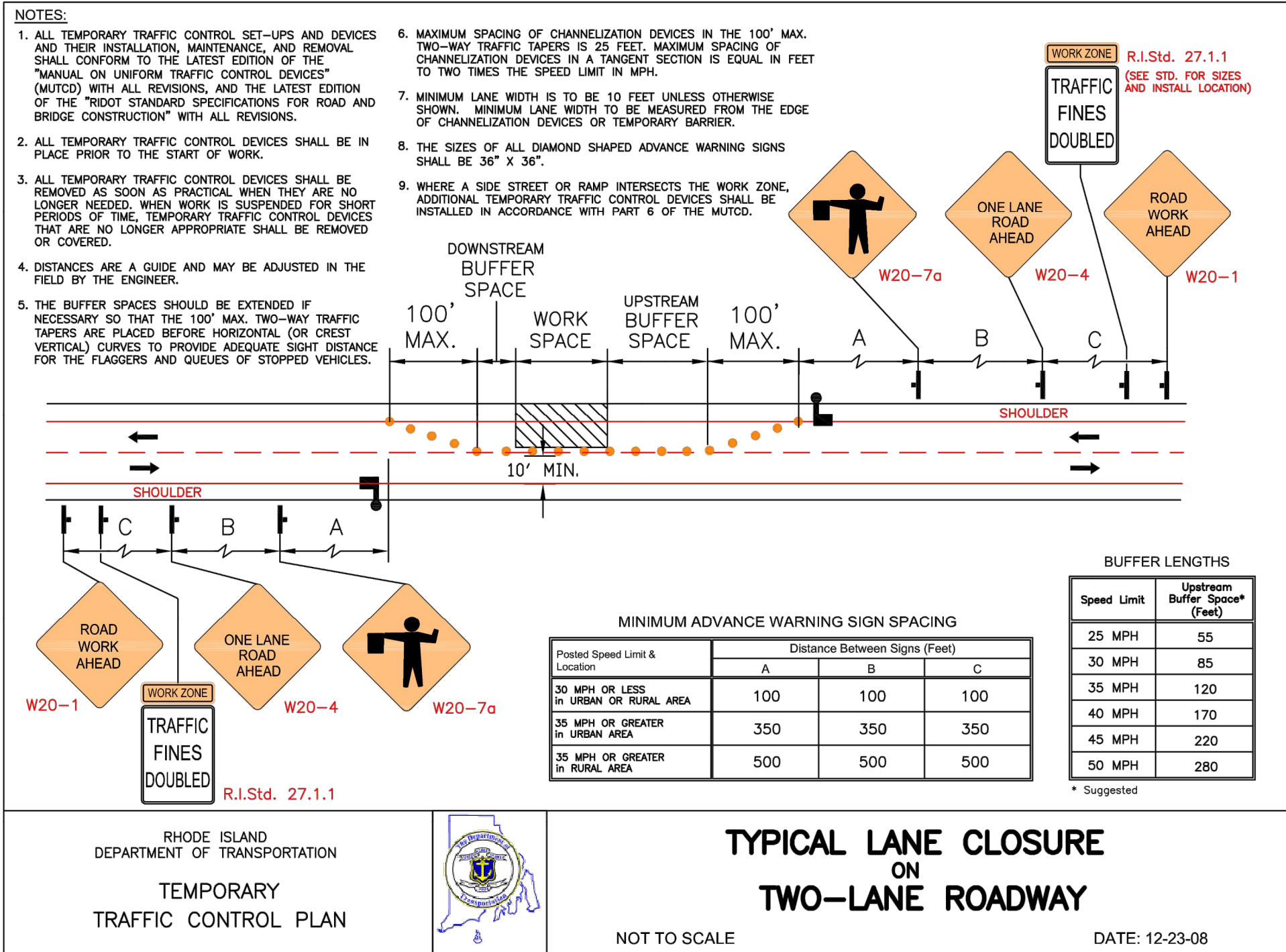
* Speed category to be determined by highway agency

** The column headings A, B, and C are the dimensions shown in Figures 6H-1 through 6H-46. The A dimension is the distance from the transition or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the TTC zone. The "third sign" is the sign that is furthest upstream from the TTC zone.)

Formulas for Determining Taper Length

Speed (S)	Taper Length (L) in feet
40 mph or less	$L = \frac{WS^2}{60}$
45 mph or more	$L = WS$

Where: L = taper length in feet
W = width of offset in feet
S = posted speed limit, or off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph



RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

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OF: 12

SCALE: NOT TO SCALE

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

POTTER STREET AT
WARREN AVENUE PARKING LOT
EAST PROVIDENCE
RHODE ISLAND
TEMP. TRAFFIC CONTROL PLAN