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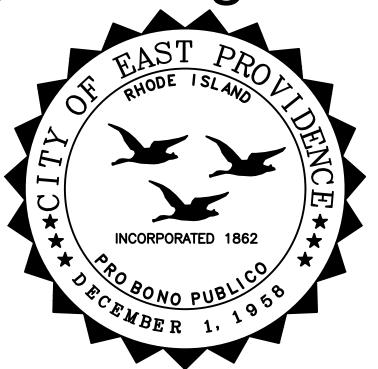
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CONSTRUCTION PLAN

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City of East Providence Department of Public Works Engineering Division



PROPOSED PARKING AREA Potter Street at Warren Avenue East Providence, Rhode Island VOLUME 1 OF 2

August 2023



INCLUDING ALL REVISIONS.

R.I. STANDARD SPECIFICATIONS AND STANDARD DETAILS

SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION,

STANDARD DETAILS FOR THIS PROJECT ARE R.I. STANDARD

DETAILS, 1998 EDITION, WITH ALL REVISIONS.

SPECIFICATIONS TO GOVERN THIS PROJECT ARE THE R.I. STANDARD

DECEMBER 2022, WITH ALL REVISIONS AND THE STATE AND FEDERAL

SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.

ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MANUAL

ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), 2009 EDITION,

BASE OF LEVELS VERTICAL DATUM: NAVD 88

LAYOUT PLAN
SCALE: 1" = 500'

PHODE ISLAND STATE DLANE NAD 92 WWW.BETA-In

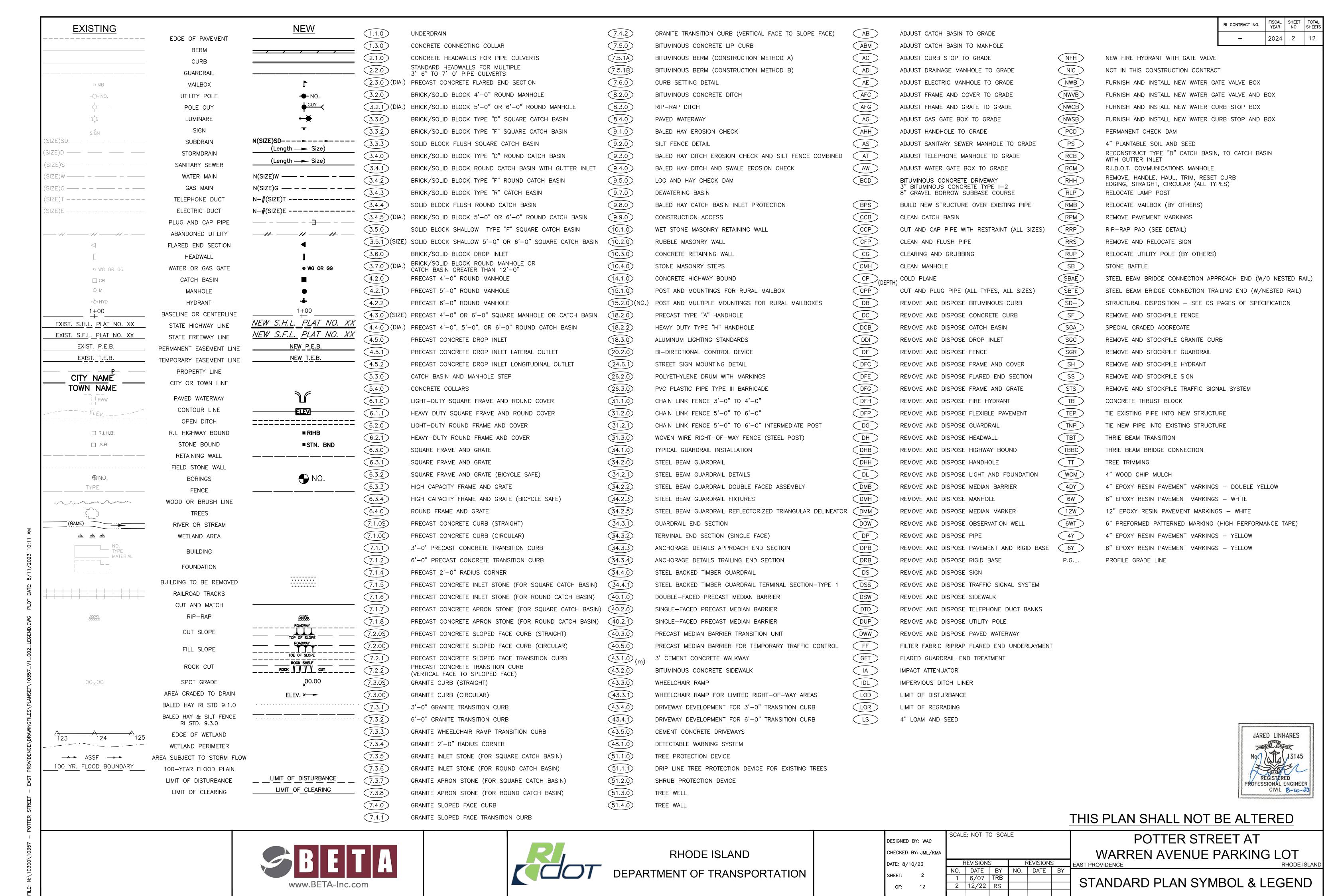
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Total Sheets _____12

No. 13145

REGISTERED
PROFESSIONAL ENGINEER
CIVIL 8-10-23

HORIZONTAL DATUM: RHODE ISLAND STATE PLANE, NAD 83



- 2. THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL AS FAR AWAY AS POSSIBLE FROM THE EDGE OF THE TRAVEL LANE SO AS NOT TO CAUSE A SAFETY HAZARD, IN ACCORDANCE WITH SECTION 106.05 OF THE R.I.D.O.T. STANDARD SPECIFICATION, LATEST EDITION. EQUIPMENT AND MATERIAL SHALL NOT BE STORED IN AREAS DESIGNATED FOR STORMWATER INFILTRATION OR OUTSIDE THE L.O.D. WITHOUT WRITTEN PERMISSION FROM THE ENGINEER.
- 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE EXISTING CONDITIONS ARE NOT DISTURBED OR OBLITERATED BEFORE SURVEY GROUND CONTROL POINTS ARE LOCATED, VERIFIED, AND DEEMED ADEQUATE FOR CONSTRUCTION LAYOUT. THE CONSTRUCTION LAYOUT SHALL BE PROVIDED IN SUFFICIENT DETAIL, THEREBY ENABLING THE CONTRACTOR TO CONSTRUCT THE PROJECT IN CONFORMITY WITH THE PLANS AND SPECIFICATIONS. SURVEY WILL BE PROVIDED BY THE CONTRACTOR. THE CONTRACTOR SHALL NOT BEGIN CONSTRUCTION ACTIVITIES UNTIL ADEQUATE SURVEY GROUND CONTROL POINTS HAVE BEEN ESTABLISHED, TIED DOWN, AND VERIFIED IN WRITING BY THE CONTRACTOR'S PROFESSIONAL LAND SURVEYOR.
- 4. ALL R.I. STD. 9.9.0 CONSTRUCTION ACCESS ROADS SHALL BE CONSTRUCTED PRIOR TO ANY ROADWAY ACCEPTING CONSTRUCTION TRAFFIC.
- 5. THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL ITEMS WILL BE DETERMINED BY THE CONTRACTOR TO MEET THE REQUIREMENTS OF SECTION 907.
- 6. ALL SIDEWALK AND DRIVEWAYS DESIGNATED FOR REPLACEMENT SHALL BE CUT AND MATCHED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
- 7. ASPHALT EMULSION TACK COAT SHALL BE PLACED PRIOR TO PAVEMENT PLACEMENT ON THE CONCRETE BASE OR COLD PLANED PAVEMENT, AND ON ANY NEW COURSE WHICH HAS BEEN OPEN TO TRAFFIC, OR ANY NEW COURSE WHICH HAS BEEN EXPOSED FOR MORE THAN 7 DAYS, AND/OR AS DIRECTED BY THE ENGINEER. IT SHALL ALSO BE APPLIED TO VERTICAL PAVEMENT FACES BETWEEN ADJOINING PAVEMENT SECTIONS. ALL APPLICATIONS ON BOTH HORIZONTAL AND VERTICAL SURFACES SHALL BE INCIDENTAL TO THE APPLICABLE PAVEMENT ITEMS.
- 8. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE SHALL BE STRICTLY ADHERED TO IN ALL AREAS. IN ADDITION TO THOSE AREAS SPECIFICALLY DESIGNATED ON THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND PLACING, AT ITS OWN EXPENSE, PLANTABLE SOIL AND SEED IN AREAS WHICH ARE OUTSIDE OF THE PROJECT'S AREAS OF DISTURBANCE AND WHICH ARE IMPACTED BY CONSTRUCTION OPERATIONS INCLUDING THOSE AREAS WHERE VEHICLES, EQUIPMENT AND MATERIALS ARE STORED.
- 9. THE CONTRACTOR WILL $\underline{\mathsf{NOT}}$ BE ALLOWED TO STOCKPILE REMOVED PAVEMENT MATERIALS WITHIN THE PROJECT LIMITS.
- 10. CLEANING AND SWEEPING OF PAVEMENT WILL INCLUDE REMOVAL OF ALL PAVEMENT DEBRIS PRIOR TO THE PLACEMENT OF EACH BITUMINOUS PAVEMENT LIFT. ALL CLEANING AND SWEEPING SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER. CLEANING WITH COMPRESSED AIR SHALL ONLY BE ALLOWED WITH THE APPROVAL OF THE ENGINEER.
- 11. PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE AS SHOWN ON THE PLANS AND SHOP DRAWINGS OR AS MODIFIED BY THE ENGINEER.
- 12. THE COORDINATE SYSTEM, IF SHOWN, IS THE RHODE ISLAND STATE PLANE COORDINATE SYSTEM.
- 13. PAVEMENT OPERATIONS FOR CURBED SECTIONS: IN AREAS WHERE CURBING IS SET TO FINISH LINE AND GRADE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE THE SENSOR AND SKY—TYPE DEVICE FOR AUTOMATIC GRADE CONTROL, BUT WILL BE ALLOWED TO MANUALLY ADJUST THE BITUMINOUS PAVER FOR CONTROLLING GRADE.
- 14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ROADWAYS FREE OF DEBRIS RESULTING FROM THEIR CONSTRUCTION OPERATIONS. ALL DEBRIS SHALL BE REMOVED TO MAINTAIN THE SAFE TRAVEL OF THE PUBLIC AT NO ADDITIONAL COST TO THE STATE.
- 15. NO FUEL STORAGE, VEHICLE REFUELING, OR EQUIPMENT STORAGE SHALL TAKE PLACE IN DESIGNATED WETLANDS, NOR WITHIN 100' OF ANY WATER BODY. THIS REQUIREMENT SHALL NOT SUPERSEDE ANY FEDERAL, STATE OR LOCAL LAW, ORDINANCE, RULE OR REGULATION THAT APPLIES TO THE SAME, UNLESS THIS REQUIREMENT IS MORE STRINGENT THAN SAID LAW, ORDINANCE, RULE OR REGULATION.
- 16. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT AT THE END OF FINAL PAVING OPERATIONS, FLOW TO NEW AND EXISTING DRAINAGE STRUCTURES HAS BEEN PROPERLY ESTABLISHED AND THAT NO ISOLATED DEPRESSIONS REMAIN. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; ANY CORRECTIVE ACTION SHALL BE CONSIDERED INCIDENTAL TO PAVING AND COLD PLANING OPERATIONS.
- 17. ALL EMBANKMENTS SHALL BE PLACED IN HORIZONTAL LAYERS NOT EXCEEDING 12" (AFTER COMPACTION) AND SHALL BE COMPACTED AS SPECIFIED BEFORE THE NEXT LAYER IS PLACED. ALSO, EMBANKMENT CONSTRUCTION SHALL CONFORM TO SECTION 202.03.2 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- 18. IF THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE, AS DESIGNATED ON THE COVERSHEET, THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAY SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.
- 19. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS STATED IN THE ENVIRONMENTAL APPROVALS ISSUED FOR THE PROJECT FROM THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM). AND/OR THE ARMY CORPS OF ENGINEERS (ACOE). AND/OR THE COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC). COPIES OF EACH OF THESE PERMITS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH THESE CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).

GENERAL NOTES (CONTINUED):

- 20. FOR ALL PROJECTS INVOLVING KNOWN SITE REMEDIATION ISSUES, THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE CONSTRUCTION RELATED PROVISIONS, CONDITIONS, AND STIPULATIONS OF ANY REMEDIAL ACTION WORK AND/OR SOIL MANAGEMENT PLANS DEVELOPED FOR THE PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH COMPLIANCE WITH THESE DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
- 21. NO UNPROTECTED CONSTRUCTED FEATURE MAY PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. HEADWALL, DRAINAGE INLET, ETC.
- 22. THE REMAINING SECTION OR STUB OF A BREAKAWAY BASE MAY NOT PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. SIGN POSTS, LIGHT POLES, FIRE HYDRANTS, ETC.

DRAINAGE AND EROSION CONTROL NOTES:

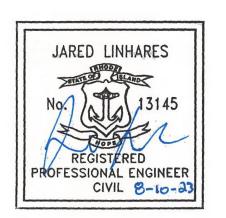
- 1. THE CONTRACTOR IS REQUIRED TO ADHERE WITH THE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIPDES GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE—SPECIFIC SWPPP FOR THIS PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MODIFYING THE SWPPP AS SITE CONDITIONS WARRANT. A COPY OF THE SWPPP MUST BE ON—SITE AT ALL TIMES. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS.
- 2. NO UNDISTURBED AREAS SHALL BE GRUBBED OF EXISTING VEGETATION AFTER OCTOBER 15 OF ANY CALENDAR YEAR OR DURING ANY PERIOD OF FULL OR LIMITED WINTER SHUTDOWN. ALL DISTURBED SOILS EXPOSED PRIOR TO OCTOBER 15 OF ANY CALENDAR YEAR SHALL BE SEEDED OR PROTECTED BY THAT DATE. ANY SUCH AREAS THAT DO NOT HAVE ADEQUATE VEGETATIVE STABILIZATION, AS DETERMINED BY THE RESIDENT ENGINEER OR ENVIRONMENTAL INSPECTOR, BY NOVEMBER 15 OF ANY CALENDAR YEAR, MUST BE STABILIZED THROUGH THE USE OF EROSION CONTROL MATTING OR HAY MULCH, IN ACCORDANCE WITH SPECIFICATIONS CONTAINED WITHIN THE R.I. SOIL EROSION AND SEDIMENT CONTROL HANDBOOK. IF WORK CONTINUES WITHIN ANY OF THESE AREAS DURING THE PERIOD FROM OCTOBER 15 THROUGH APRIL 15, CARE MUST BE TAKEN TO ENSURE THAT ONLY THE AREA REQUIRED FOR THAT DAY'S WORK IS EXPOSED, AND ALL ERODIBLE SOIL MUST BE RESTABILIZED WITHIN 5 WORKING DAYS. ANY WORK TO CORRECT PROBLEMS RESULTING FROM FAILURE TO COMPLY WITH THIS PROVISION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THERE WILL BE NO SEPARATE PAYMENT FOR THIS PROVISION, IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN 2 WEEKS OF FINAL GRADING.
- 3. STOCKPILES OF MATERIAL SHALL NOT BE LOCATED WITHIN REGULATED WETLANDS OR BUFFER ZONE AREAS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND STOCKPILES OF ERODIBLE MATERIAL SHALL ALSO BE SEEDED AND RINGED WITH APPROPRIATE SEDIMENT AND EROSION CONTROL MEASURES TO STABILIZE. STOCKPILES OF CONTAMINATED MATERIALS MUST BE PLACED ON TOP OF A POLY-ETHYLENE SHEET AND COVERED AT ALL TIMES UNLESS IT IS AN ACTIVE WORKING PILE.
- 4. IF THE PLANS INCLUDE SPECIFIC AREAS FOR PLACEMENT OF CONSTRUCTION DEWATERING BASINS AND/OR EQUIPMENT AND MATERIALS STORAGE AND STOCKPILING, AND IF THE CONTRACTOR ELECTS TO UTILIZE ANY OTHER AREAS FOR THESE PURPOSES, THIS SHALL BE APPROVED BY THE ENGINEER ONLY AFTER OBTAINING ANY NECESSARY PERMITS AND/OR PERMIT MODIFICATIONS FROM THE APPROPRIATE REGULATORY AUTHORITY(IES). ANY PERMITTING REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE ACCOMPLISHED AT NO COST TO THE STATE. THE ENGINEER WILL COORDINATE SUBMISSION OF ANY REQUIRED PERMIT APPLICATION MATERIALS WITH THE R.I.D.O.T. ENVIRONMENTAL DIVISION.
- 5. SURFACE EROSION CONTROL MATTING SHALL BE USED TO STABILIZE PLANTABLE SOIL AND/OR LOAM IN ALL DITCHES, ON ALL SLOPES ADJACENT TO WETLANDS AND WETLAND PERIMETERS, AND ON ALL SLOPES WITHIN WATER QUALITY BASINS. JUTE MESH IN DITCHES SHALL EXTEND TO AN ELEVATION 2 FEET ABOVE THE BOTTOM OF THE DITCH.
- 6. SEEDING ON ALL SLOPES 3 TO 1 OR STEEPER SHALL CONSIST OF THE FOLLOWING APPLICATIONS UNLESS CHANGED IN THE CONTRACT.
 - a. SEEDING TYPE I.
 - b. ADHESIVE MULCH STABILIZER
- 7. UNVEGETATED SLOPES SHALL NOT BE UNATTENDED OR EXPOSED FOR PERIODS IN EXCESS OF 2 WEEKS OR THROUGH THE INACTIVE WINTER SEASON.
- 8. PRIOR TO CONSTRUCTION OPERATIONS, THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL CATCH BASINS AND FLUSHING THE PIPES, AND THEN VERIFYING THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING PIPES AND/OR STRUCTURES WHICH ARE TO BE CONNECTED. ANY VARIATION FOUND FROM THE PLANS MUST BE BROUGHT TO THE ENGINEER'S ATTENTION.
- 9. ALL DRAINAGE AND UTILITY STRUCTURES WITHIN THE PAVED ROADWAY SHALL BE ADJUSTED TO GRADE WITH THE SURROUNDING PAVEMENT PRIOR TO THE WINTER
- 10. DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE EFFICACY OF THE DRAINAGE SYSTEM. ONCE CONSTRUCTION IS COMPLETED THE CONTRACTOR IS RESPONSIBLE FOR CLEANING ALL CATCH BASINS AND FLUSHING ALL PIPES OF ANY CONSTRUCTION RELATED DEBRIS AT NO ADDITIONAL COST.
- 11. CATCH BASIN RIM GRADES FOR STRUCTURES NOT IN A TRAVEL LANE NOTED ON PLANS ARE DEPRESSED 0.1' LOWER THAN THE GUTTER GRADE. RIM ELEVATIONS SHOWN ARE FINAL GRADES. THE CONTRACTOR SHALL PLACE FRAMES AND GRATES 0.1' BELOW THE GRADE CONSTRUCTED IN THIS CONTRACT OR AS DIRECTED BY THE ENGINEER.
- 12. PROVISIONS FOR CLEARING TO ACCESS OUTFALLS DURING THE CLEANING AND FLUSHING OF THE CLOSED DRAINAGE SYSTEM SHALL STRICTLY ADHERE TO THE PLANS AND SPECIFICATIONS.
- 13. THE CONTRACTOR SHALL INSTALL ALL SEDIMENT AND EROSION CONTROL DEVICES FOR OUTLET PROTECTION PRIOR TO CLEANING AND FLUSHING STORM WATER DRAINAGE. SEDIMENT AND EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ALL FLUSHED SEDIMENTS ARE REMOVED. AT ALL OUTFALL LOCATIONS WHERE PIPES ARE TO BE CLEANED AND FLUSHED, OUTLET PROTECTION (R.I. STD. 9.1.0 OR 9.3.0) SHALL BE INSTALLED TO TRAP SEDIMENTS. THESE SEDIMENTS SHALL THEN BE REMOVED AND DISPOSED OF LEGALLY BEFORE THE OUTLET PROTECTION DEVICES ARE REMOVED. IF OUTLET PROTECTION AT THE OUTFALL IS NOT FEASIBLE, THEN THE OUTLET PIPE OF THE LAST DRAINAGE STRUCTURE TO BE CLEANED SHALL BE PLUGGED TO CAPTURE ALL MATERIALS FLUSHED FROM PIPES. AFTER THE MATERIALS ARE REMOVED FROM THE DRAINAGE STRUCTURE, THE OUTLET SHALL BE UNPLUGGED TO RESUME NORMAL FUNCTIONING
- 14. R.I. STD. 9.8.0 BALED STRAW INLET PROTECTION SHALL BE INSTALLED AT ALL CATCH BASINS AND INLETS WHENEVER SUBBASE IS EXPOSED, AND SHALL REMAIN IN PLACE UNTIL THE ABUTTING GROUND SURFACES ARE STABILIZED.
- 15. WHERE BALED STRAW INLET PROTECTION AND SILT FENCES ARE USED AT CATCH BASINS, THEY SHALL BE REMOVED AT THE END OF THE PROJECT OR AS DIRECTED BY THE ENGINEER IN ORDER TO PREVENT CLOGGING OF THE INLET.

DRAINAGE AND EROSION CONTROL NOTES (CONTINUED):

- 16. DETENTION AND RETENTION BASINS MAY BE ROUGH GRADED AND STABILIZED WITH VEGETATION AND/OR OTHER EROSION CONTROL MEASURES AS REQUIRED BY THE ENGINEER PRIOR TO USE AS TEMPORARY SEDIMENTATION BASINS DURING PROJECT CONSTRUCTION. FINAL BASIN CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL SOURCES OF SEDIMENT HAVE BEEN REMOVED AND INFILTRATION IS REESTABLISHED, FINAL ROADSIDE VEGETATION IS ESTABLISHED AND USE OF TEMPORARY BASINS IS NO LONGER REQUIRED TO COMPLY WITH THE PLANS, SPECIFICATIONS, AND PERMITS. ANY ISSUES RELATING TO EROSION AND/OR SEDIMENT TRANSPORT INTO WETLAND AREAS RESULTING FROM SUCH USE OF SEDIMENTATION BASINS DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY CORRECTIVE ACTION AND COSTS REQUIRED TO RESOLVE SUCH ISSUES IS THE RESPONSIBILITY OF THE CONTRACTOR.
- 17. THE TOE OF ANY FILL SLOPE IS TO REMAIN AT LEAST 1' INSIDE OF ALL EROSION CONTROLS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR COVER ANY PORTION OF THE EROSION CONTROL MEASURES WITH MATERIAL. ANY MATERIAL THAT IS PLACED ON ANY EROSION CONTROLS BY THE CONTRACTOR, OR ANY AGENT OF THE CONTRACTOR, SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR, AND ANY NECESSARY REPAIRS TO THE EROSION CONTROLS ACCOMPLISHED.
- 18. PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES, EROSION AND SEDIMENTATION CONTROLS SHALL BE INSTALLED AT THOSE AREAS INDICATED ON THE PLANS. CLEARING MAY OCCUR PRIOR TO INSTALLATION OF SUCH CONTROLS, HOWEVER NO GRUBBING, GRADING, FILLING, OR OTHER SOIL DISTURBANCE SHALL OCCUR PRIOR TO INSTALLATION. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS.
- 19. ALL COMPOST FILTER SOCK, STRAW BALES, SILT FENCE OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS IS ESTABLISHED. IF NEEDED, TEMPORARY SEEDING CAN HELP TO MINIMIZE EROSION. TEMPORARY SEED WILL CONFORM TO R.I.D.O.T. STANDARD TEMPORARY SEED MIX.
- 20. THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE YEAR AND SHALL DO SO AT NO ADDITIONAL EXPENSE TO THE STATE.
- 21. THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE SPECIFIED IN SUBSECTION L.02.03 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- 22. ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEMS. ADDITIONAL SEDIMENT AND EROSION CONTROLS, SHALL BE INSTALLED IN ACCORDANCE WITH THE SWPPP REPORT. THESE ADDITIONAL ITEMS WILL BE PAID AT THE UNIT PRICE FOR THAT BID ITEM.
- 23. ANY OBSERVATIONS OF ILLICIT CONNECTIONS OR DISCHARGES TO RIDOT'S DRAINAGE NETWORK OR OUTFALLS SHALL BE REPORTED TO THE RIDOT STORMWATER UNIT IMMEDIATELY.

UTILITY NOTES

- 1. EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS USING THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE. BUILDING SERVICE CONNECTIONS (ELECTRIC, GAS, TELEPHONE, WATER AND SANITARY) ARE NOT SHOWN. CONTRACTOR IS TO ASSUME SERVICES ARE PRESENT TO ALL BUILDINGS.
- 2. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING DRAINAGE AND UTILITIES BOTH UNDERGROUND AND OVERHEAD BEFORE EXCAVATION BEGINS IN ACCORDANCE WITH CHAPTER 39–1.2 OF THE R.I. GENERAL LAWS ENTITLED "EXCAVATION NEAR UNDERGROUND UTILITY FACILITIES", WITH AMENDMENTS EFFECTIVE AS OF NOVEMBER 1, 2009 AND, WHEN NECESSARY, BY CONTACTING THE INDIVIDUAL UTILITY COMPANIES. EXCAVATION SHALL BE IN ACCORDANCE WITH ALL STATUTES, ORDINANCES, RULES AND REGULATIONS OF ANY APPLICABLE CITY, TOWN, STATE OR FEDERAL AGENCY. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE DIG SAFE PROGRAM. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES AND AND AND ENSURE THAT ALL UTILITIES HAVE BEEN MARKED PRIOR TO COMMENCING THEIR WORK. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANY, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.
- 3. ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE CAPPED.
- 4. EXISTING WATER SERVICES SHALL BE RECONNECTED TO THE NEW WATER MAINS.
- 5. UTILITY SERVICE CONNECTIONS SHALL BE MAINTAINED TO ALL EXISTING FACILITIES TO REMAIN.
- 6. FIRE HYDRANTS SHALL NOT BE REMOVED FROM SERVICE WITHOUT WRITTEN AUTHORIZATION FROM THE FIRE DEPARTMENT OR THE WATER AUTHORITY.
- 7. ALL NEW WATER LINES SHALL BE DISINFECTED TO THE SATISFACTION OF THE WATER AUTHORITY IN ACCORDANCE WITH THE SPECIFICATIONS.
- 8. ALL UTILITY POLE RELATED WORK SHALL BE BY OTHERS.
- 9. THE CONTRACTOR SHALL PROVIDE 72-HOUR ADVANCE NOTICE TO THE RIDOT TMC (401-222-2378) FOR WORK AROUND RIDOT OWNED INFRASTRUCTURE (DRAINAGE, LIGHTING, ITS EQUIPMENT, TOLL GANTRIES, COUNTING STATIONS, ETC.). ANY DAMAGE TO THIS INFRASTRUCTURE MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT RIDOT IN ADVANCE, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.



FISCAL SHEET TOTAL YEAR NO. SHEETS

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THIS PLAN SHALL NOT BE ALTERED





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- 2. ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- 3. ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE R.I.D.O.T. LANDSCAPE ARCHITECTURE UNIT.
- 4. COORDINATE WITH THE R.I.D.O.T. CONSTRUCTION MANAGER PRIOR TO ALL TRIMMING AND CLEARING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
- 5. ANY TOPSOIL USED AS PLANTABLE SOIL SHALL HAVE A SANDY LOAM TEXTURE RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS, AND SHALL CONFORM TO SECTION M.18 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- 6. ALL TREES AND SHRUBS SHALL BE MULCHED WITH PINE BARK MULCH IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- 7. ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE MULCHED AS A BED.
- 8. PROVIDE A MINIMUM 6'-8" BRANCHING STANDARD ON ALL TREES INSTALLED ADJACENT TO SIDEWALKS AND/OR PEDESTRIAN ACCESS AREAS.
- 9. THE CONTRACTOR SHALL PROVIDE CERTIFICATION THAT THERE ARE NO CONTAMINANTS THAT EXCEED THE R.I.D.E.M. PERMISSIBLE LEVELS IN THE SOILS USED AS LOAM OR PLANTABLE SOIL.

STRUCTURAL NOTES FOR HIGHWAY SIGNS, **LUMINAIRES AND TRAFFIC SIGNALS:**

GENERAL

1. ALL SUPPORT DESIGNS AND ASSOCIATED SHOP DRAWING REVIEWS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION AND REVISIONS, OF THE AASHTO LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINARIES, AND TRAFFIC SIGNALS, INCLUDING THE LATEST INTERIM SPECIFICATIONS, EXCEPT AS MODIFIED HEREIN.

CONSTRUCTION DRAWINGS AND DETAILS

- 1. THE FOLLOWING NOTES SHALL BE INCLUDED ON ALL PLANS AND/OR SHOP DRAWINGS IN REFERENCE TO ANCHOR BOLTS:
 - "PRETENSIONING OF ALL ANCHOR NUTS IS REQUIRED, AND SHALL BE ACCOMPLISHED BY TIGHTENING TO 1/6TH TURN BEYOND THE SNUG-TIGHT POSITION."
 - THE MAXIMUM CLEARANCE BETWEEN THE BOTTOM OF THE LEVELING NUTS AND THE TOP OF THE CONCRETE IS CRITICAL AND SHALL NOT EXCEED THE AMOUNT SPECIFIED ON THIS DRAWING."
- THE USE OF GROUT UNDER BASE PLATES SHALL GENERALLY NOT BE PERMITTED. IF SPECIFIC CONDITIONS WARRANT ITS USE, THE GROUT SHALL NOT BE CONSIDERED LOAD CARRYING; LOADS SHALL BE DIRECTLY SUPPORTED BY THE ANCHOR BOLTS. ADEQUATE DRAINAGE SHALL BE PROVIDED.
- THE DAMPENING EFFECTS OF VIBRATION MITIGATION DEVICES SHALL NOT BE CONSIDERED IN THE DESIGN OF STRUCTURAL SUPPORTS FOR SIGNS AND TRAFFIC SIGNALS. IF THE CONTRACTOR CHOOSES TO USE THESE DEVICES FOR WARRANTY PURPOSES, THE TYPE OF DEVICES PROPOSED SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO FABRICATION OF SUPPORTS.

TRAFFIC SIGNAL NOTES:

- ALL SALVAGED TRAFFIC SIGNAL EQUIPMENT SHALL BE DELIVERED TO THE R.I.D.O.T. MAINTENANCE HEADQUARTERS, 360 LINCOLN AVENUE, WARWICK, RHODE ISLAND, 02888. THE COST FOR DELIVERY IS CONSIDERED INCIDENTAL TO THE WORK.
- 2. BACK PLATES SHALL BE INSTALLED ON ALL TRAFFIC SIGNAL HEADS.
- THE CONTRACTOR SHALL SUPPLY AND INSTALL ON THE UPPER LEFT HAND CORNER OF THE BACK OF THE CONTROLLER CABINET DOOR A LAMINATED INTERSECTION GRAPHIC AND TABLE DEPICTING THE TRAFFIC DETECTOR RELAY CHANNEL ASSIGNMENTS. THE DIAGRAM SHALL BE A GRAPHIC OF THE INDIVIDUAL INTERSECTION ORIENTED SIMILAR TO THE PLANS SHOWING THE LOCATIONS OF EACH OF THE LOOP DETECTORS. THE DIAGRAM SHALL, AT A MINIMUM, INCLUDE DETECTOR NUMBERS, STREET NAME LABELS, NORTH ARROW, AND CONTROLLER CABINET LOCATION. THE ASSIGNMENT INFORMATION SHALL BE INCLUDED IN A TABLE WHICH SHALL INCLUDE, AT A MINIMUM, THE APPROACH NAME, DETECTOR NUMBER, TERMINAL NUMBER, DETECTOR RACK SLOT NUMBER, RELAY NUMBER, RELAY CHANNEL NUMBER, AND PHASE ASSOCIATED WITH EACH DETECTOR.
- TRAFFIC CONTROLLER CABINETS, UNLESS OTHERWISE NOTED, SHALL BE NEMA TS2 TYPE 1 CABINET SIZE 6 ("P" TYPE) WITH NOMINAL DIMENSIONS OF 52"Hx44"Wx24"D.
- 5. ALL DELAY AND EXTENSION TIMES, AS CALLED FOR ON THE PLANS, FOR PROPOSED LOOP DETECTORS SHALL BE PROGRAMMED IN THE TRAFFIC SIGNAL CONTROLLER AND NOT THE DETECTOR RELAY.
- INSULATED GROUND WIRE SHALL BE PLACED IN ALL PVC CONDUITS AND SHALL BE BONDED TO GROUND RODS IN ACCORDANCE WITH SECTION T.03 OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- 7. THE FINAL POSITION OF SIGNAL HEADS, PEDESTRIAN PUSHBUTTONS, DETECTORS, AND STOP LINE AND CROSSWALK PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER IN THE FIELD ACCORDING TO ACTUAL INTERSECTION CHARACTERISTICS.
- 8. A 2' MINIMUM BUFFER SHALL BE PROVIDED BETWEEN THE CURB AND ALL LATERAL OBSTRUCTIONS (INCLUDING ALL SIGNAL POLES AND TRAFFIC/PEDESTRIAN SIGNAL HEADS) TO PROVIDE ADEQUATE CLEARANCE FOR TURNING VEHICLES.
- 9. ALL FOUNDATIONS MUST HAVE CONES OR BARRELS BOLTED TO FOUNDATION BASES UNTIL ACTUAL POLE IS INSTALLED.
- 10. WHEN PLACING TRAFFIC SIGNAL HANDHOLES OR CONDUIT IN EXISTING PORTLAND CEMENT CONCRETE SIDEWALKS, THE ENTIRE SIDEWALK SQUARE OF CONCRETE SHALL BE REPLACED IN ACCORDANCE WITH R.I. STD. 43.1.0. NO PATCHES WILL BE ALLOWED.
- 11. ALL PEDESTRIAN PUSHBUTTONS SHALL BE COMPLIANT WITH "THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES" (ADAAG) AND SHALL INCLUDE A PRESSURE-ACTIVATED (NON-MOVING) BUTTON. SIGNS APPLICABLE TO PUSHBUTTON ACTUATION SHALL BE INSTALLED SUCH THAT THE CROSSING ASSIGNED TO EACH BUTTON IS CLEARLY INDICATED. IF SITE CONDITIONS DO NOT ALLOW PEDESTRIAN PUSHBUTTONS TO BE INSTALLED WHERE CALLED FOR ON THE PLANS, THE R.I.D.O.T. TRAFFIC ENGINEERING UNIT SHALL BE CONSULTED WITH THROUGH AN R.F.I. PRIOR TO INSTALLING THE PUSHBUTTONS. THE FINAL PLACEMENT OF ALL PEDESTRIAN PUSHBUTTONS SHALL BE IN ACCORDANCE WITH ADAAG AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- 12. ALL LOOP DETECTORS SHALL BE CENTERED WITHIN EACH LANE AS DELINEATED, UNLESS OTHERWISE DIMENSIONED ON PLANS.
- 13. ALL LOOP DETECTORS SHALL BE CUT INTO THE FINAL PAVEMENT SURFACE COURSE.
- 14. TRAFFIC SIGNAL CONTROLLERS AND CABINETS SHALL BE PROGRAMMED AND WIRED SO THAT ANY FIRE PRE-EMPTION SHALL OVERRIDE MANUAL (PUSH BUTTON) OPERATION.
- 15. THE CONTRACTOR SHALL WORK CONTINUOUSLY TO RESTORE TRAFFIC SIGNAL OPERATION TO ITS INTENDED PURPOSE WHEN REPLACING THE TRAFFIC SIGNAL EQUIPMENT. A POLICE DETAIL IS REQUIRED TO DIRECT TRAFFIC AT THE INTERSECTION AT ALL TIMES WHEN THE TRAFFIC SIGNAL IS INOPERATIVE. AT NO TIME SHALL THE CONTRACTOR LEAVE THE SITE BEFORE RESTORING FULL TRAFFIC OPERATIONS.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

- ALL MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL SETUPS, SIGNS, CHANNELIZING DEVICES, ETC., SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. LATEST EDITION.
- ALL SIGN MOUNTINGS FOR TEMPORARY AND CONSTRUCTION SIGNS SHALL BE IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST
- THE CONTRACTOR SHALL COVER ALL EXISTING AND/OR TEMPORARY SIGNS THAT ARE NOT RELEVANT TO THE TRAFFIC CONTROL REQUIRED DURING ANY PARTICULAR STAGE OF THE CONTRACT.
- ADVANCE FLAGPERSON SIGNS (W20-7A) SHALL BE USED IN ADVANCE OF ANY POINT AT WHICH A FLAGPERSON OR A POLICE OFFICER HAS BEEN STATIONED TO CONTROL TRAFFIC. WHEN NEEDED. AN APPROPRIATE DISTANCE MESSAGE MAY BE DISPLAYED ON A SUPPLEMENTAL PLATE (24"x18") BELOW THE FLAGPERSON SYMBOL SIGN. THE SIGN SHALL BE PROMPTLY REMOVED OR COVERED WHENEVER THE FLAGPERSON IS NOT AT THE STATION.
- POLICE OFFICERS AND FLAGPERSONS SHALL BE UTILIZED AS OUTLINED IN SECTIONS 913 & 914 OF THE RI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- POLYETHYLENE DRUMS SHALL BE UTILIZED AS A CHANNELIZING DEVICE WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN BEYOND WORKING HOURS WHEN NO WORKERS ARE PRESENT. CONES SHALL BE UTILIZED WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN ONLY DURING WORKING HOURS AND IS SUBSEQUENTLY BROKEN DOWN AT THE END OF THE WORKDAY.
- ARROW PANELS SHALL BE SET IN THE FLASHING FOUR CORNERS CAUTION MODE UNLESS UTILIZED FOR A MERGING TAPER. ARROW PANELS SET IN THE FLASHING ARROW MODE SHALL NOT BE UTILIZED FOR LANE SHIFTS.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER WORKZONE TRAFFIC CONTROL DEVICES THAT ARE DAMAGED OR REQUIRE RELOCATION SHALL BE REPLACED AND / OR RELOCATED UNDER THE PAY ITEM FOR "MAINTENANCE AND MOVEMENT TRAFFIC PROTECTION."
- THE PRIVATE VEHICLES OF CONSTRUCTION WORKERS SHALL NOT BE PARKED ON THE TRAVEL LANES OR SHOULDERS. THEY MAY BE PARKED WITHIN THE STATE RIGHT-OF-WAY ONLY IN AREAS BEYOND THE OUTSIDE EDGE OF THE TRAVEL LANES AND/OR IN AREAS APPROVED BY THE ENGINEER.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC, AND SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER APPROPRIATE.
- THE INTENDED VEHICLE PATHS THROUGH EACH WORK ZONE SHALL BE CLEARLY MARKED AT ALL TIMES. APPROVED PAVEMENT MARKINGS SHALL BE INSTALLED BEFORE THE END OF THE WORK SHIFT ON ALL COLD-PLANED AND NEW ROADWAY SURFACES THAT WILL BE OPENED TO TRAFFIC AT THE END OF THE SHIFT. FAILURE TO COMPLY WILL RESULT IN AN ASSESSMENT OF A CHARGE AS OUTLINED IN SECTION 937 OF THE RI STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.



FISCAL SHEET TOTAL YEAR NO. SHEETS

2024 4 12

RI CONTRACT NO.

THIS PLAN SHALL NOT BE ALTERED





RHODE ISLAND DEPARTMENT OF TRANSPORTATION DESIGNED BY: WAC CHECKED BY: JML/KM DATE: 8/10/23 SHEET: OF:

REVISIONS REVISIONS NO. | DATE NO. | DATE | 1 4/07 TRB 4 | 12/22 | JRP 2 | 3/10 | RBH

3 4/14 MLP

SCALE: NOT TO SCALE

POTTER STREET AT WARREN AVENUE PARKING LOT

STANDARD NOTES - 2

STA

SSD

TAN

TEB

TEMP

TOC

TOS

TP

TYP

VAR

VERT

VC

WB

WCR

WD

CB

CBCI

CIP

CL

COND

CAP

CMP

CPP

CSP

CULV

CI

CS

DI

EL (OR ELEV)

FΜ

F&C

F&G

GIP

GG

GI

HDWL

HYD

INV

LP

LPS

МН

PVC

PWW

R&D

RCP

STATION

TANGENT

TEMPORARY

TOP OF CURB

TOP OF WALL

TYPICAL

VARIABLE

VERTICAL

WOOD

CULVERT

CURB INLET

CURB STOP

ELEVATION

GAS GATE

HEADWALL

HYDRANT

MANHOLE

LIGHT POLE

GUTTER INLET

FORCE MAIN

DUCTILE IRON PIPE

FRAME AND COVER

FRAME AND GRATE

INVERT ELEVATION

PAVED WATER WAY

SEWER MANHOLE

TRAFFIC SIGNAL

UTILITY POLE

WATER GATE

TRAFFIC SIGNAL CONDUIT

WATER METER / WATER MAIN

VITRIFIED CLAY PIPE

SUBDRAIN

REMOVE & DISPOSE

LOW PRESSURE SERVICE CONNECTION

SANITARY SEWER OR SERVICE CONNECTION

REINFORCED CONCRETE PIPE (CLASS III UNLESS NOTED)

POLY-VINYL-CHLORIDE PIPE

GALVANIZED IRON PIPE

TOP OF SLOPE

TURNING POINT

VERTICAL CURVE

WHEELCHAIR RAMP

CATCH BASIN WITH CURB INLET

CORRUGATED ALUMINUM PIPE

CORRUGATED METAL PIPE

CORRUGATED PLASTIC PIPE

CORRUGATED STEEL PIPE

CLASS (PIPE, CONCRETE, EXCAVATION, ETC)

WEST BOUND

CATCH BASIN

CAST IRON PIPE

STOPPING SIGHT DISTANCE

TEMPORARY EASEMENT BOUNDARY

TANGENT DISTANCE OF CURVE/TRUCK PERCENTAGE

| No. | REGIS | TEREI AL EN | 3145 |
|-----|-------|-------------|------|

SIGN

TREE PROTECTION

JOB SPECIFIC LEGEND

REMOVAL AND DISPOSAL OF CURBING (ITEM 201.0436)

INLET SEDIMENT CONTROL DEVICE (SEE DETAIL ON SHT. 6)

CUTTING AND DISPOSING ISOLATED TREES AND STUMPS, ALL SIZES

REMOVE AND RELOCATE DIRECTIONAL REGULATORY AND WARNING SIGN (ITEM T15.0200)

COMPOST FILTER SOCK (SEE DETAIL ON SHT. 6)

4" EPOXY RESIN PAVEMENT MARKINGS - WHITE

BLDG

CEM

CLF

CONC

CONT

CONST

CO

DHV

DWY

EΒ

EP, EOP

ETW

EXIST

FLDSTN

FDN

GAR

GRAN

GRAV

GND

HORIZ

НМА

HSE

JCT

MB

MAX

MIN

NB

NTS

O.C.

PVMT

PEB

PCC

PRC

PS

PC

РΙ

PVI

PVT

PGL

PROJ

PROP

R&D

REM

REMOD

BUILDING

CEMENT

CENTER LINE

CONCRETE

COUNTY

DRIVEWAY

ELEVATION

FIELDSTONE

FOUNDATION

EXISTING

GARAGE

GRANITE

GROUND

HORIZONTAL

HOT MIX ASPHALT

LENGHT OF CURVE

GRAVEL

HOUSE

LEFT

IRON PIPE

JUNCTION

LOW POINT

MAIL BOX

MAXIMUM

MINIMUM

NORTH BOUND

NOT TO SCALE

PERMANENT EASEMENT BOUNDARY

POINT OF COMPOUND CURVATURE

POINT OF REVERSE CURVATURE

POINT OF VERTICAL CURVATURE

POINT OF VERTICAL TANGENCY

POINT OF VERTICAL INTERSECTION

PLANTABLE SOIL BORROW

POINT OF CURVATURE

POINT OF TANGENCY

PROFILE GRADE LINE

RADIUS OF CURVATURE

REMOVE & DISPOSE

PROPERTY LINE

POINT OF INTERSECTION

ON CENTER

PAVEMENT

PROJECT

PROPOSED

REMOVE

REMODEL

EAST BOUND

CONTINUOUS

CONSTRUCTION

CHAIN LINK FENCE

DESIGN HOURLY VOLUME

EDGE OF PAVEMENT

EDGE OF TRAVEL WAY

DELTA ANGLE (CENTRAL ANGLE OF HORIZ CURVE)

SIGN

CFS

DGC

DT

(ISCD)

RET

RRS

(S-1)

4W

RETAIN

FULL DEPTH SAWCUT

(43.1.0) (m) CEMENT CONCRETE SIDEWALK (MODIFIED)



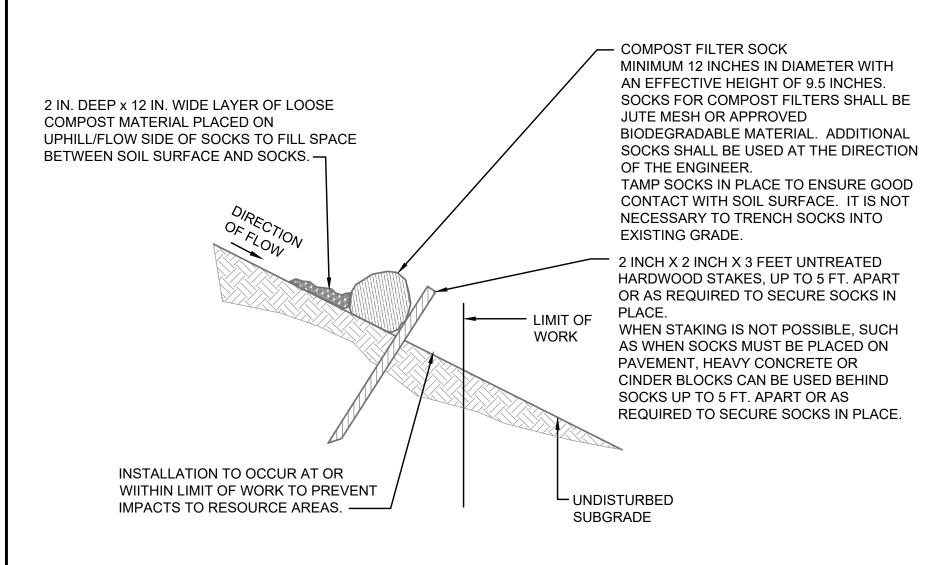


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| DATE: 8/10 |)/23 | |
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| OF: | 12 | |

| ALE: NOT TO SCALE | | | | | | POTTER STREET AT | | |
|-------------------|----------|----|-----|-----------|----|-------------------------------------|--|--|
| | | | | | | WARREN AVENUE PARKING LOT | | |
| R | EVISIONS | S | F | REVISIONS | S | EAST PROVIDENCE RHODE ISLA | | |
|). | DATE | BY | NO. | DATE | BY | | | |
| | | | | | | JOB SPECIFIC SYMBOLS LEGEND & NOTES | | |

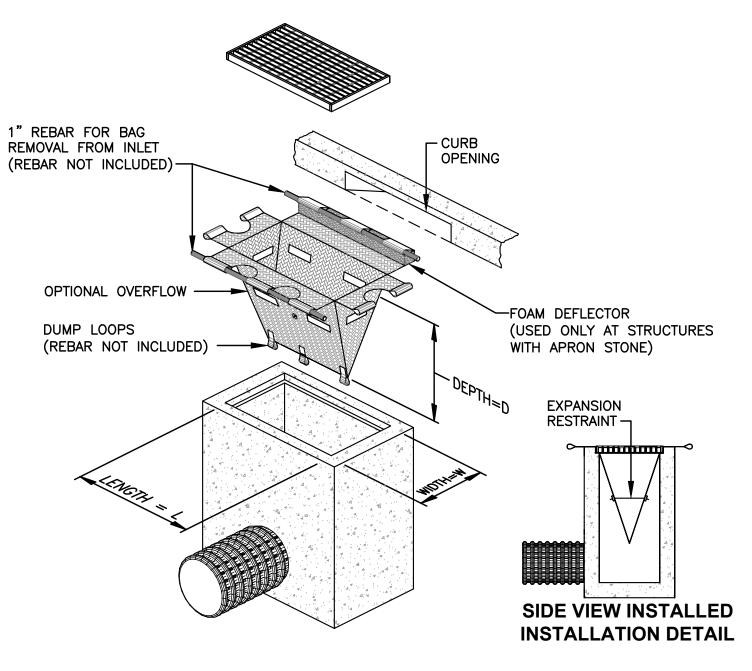
FISCAL SHEET TOTAL YEAR NO. SHEETS

2024 5 12

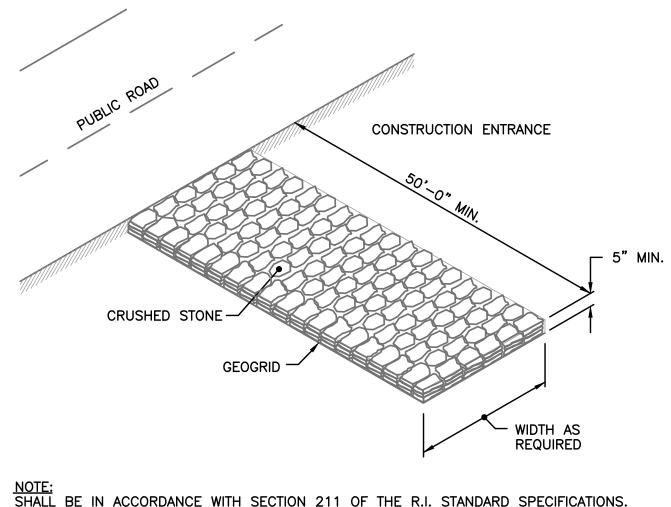


- 1. PROVIDE A MINIMUM SOCK DIAMETER OF 12 INCHES FOR SLOPES UP TO 50 FEET IN LENGTH WITH A SLOPE RATIO OF 3H:1V OR STEEPER. LONGER SLOPES OF 3H:1V MAY REQUIRE LARGER SOCK DIAMETER OR ADDITIONAL COURSING OF FILTER SOCKS TO CREATE A FILTER BERM. REFER TO MANUFACTURER'S RECOMMENDATIONS FOR SITUATIONS WITH LONGER OR STEEPER SLOPES.
- 2. INSTALL SOCKS ALONG CONTOURS AND PERPENDICULAR TO SHEET OR CONCENTRATED FLOW.
- 3. DO NOT INSTALL IN PERENNIAL, EPHEMERAL OR INTERMITTENT STREAMS.
- 4. CONFIGURE SOCKS AROUND EXISTING SITE FEATURES TO MINIMIZE SITE DISTURBANCE AND MAXIMIZE CAPTURE AREA OF STORMWATER RUN-OFF.



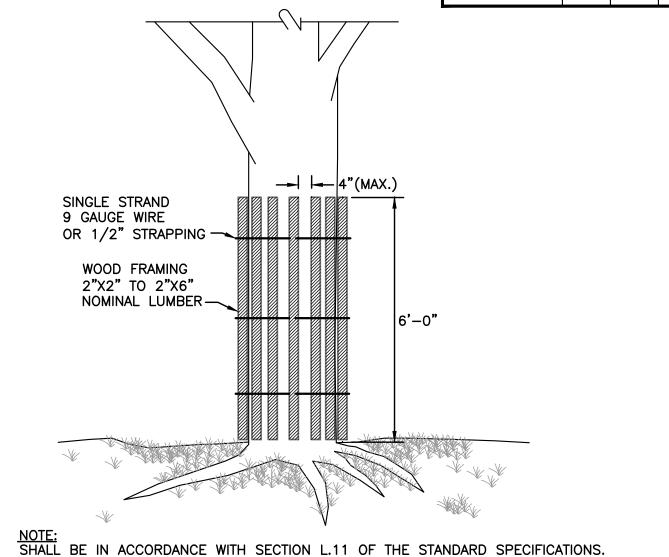








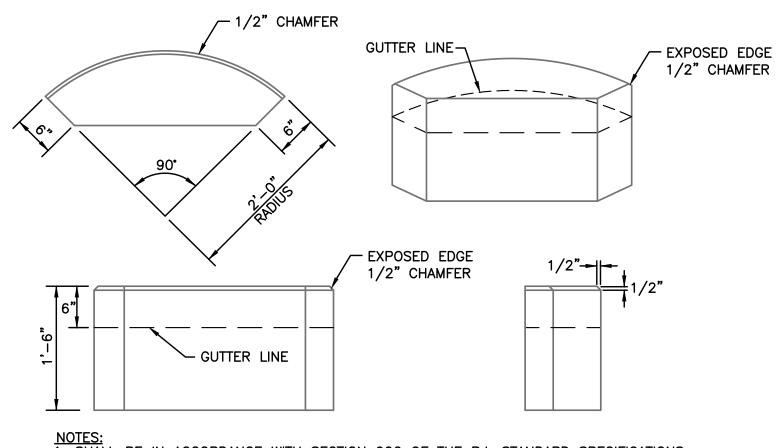




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TREE PROTECTION DEVICE NOT TO SCALE

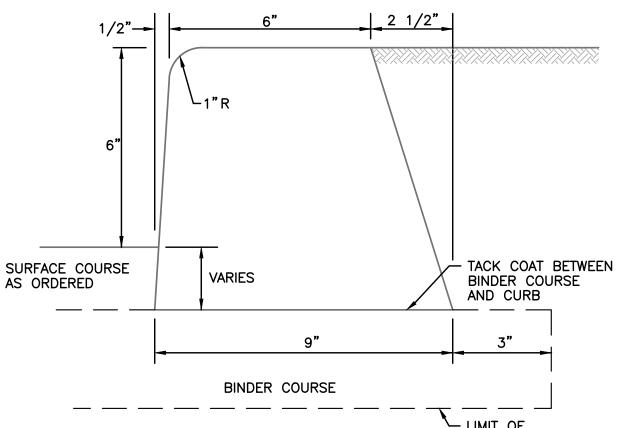


NOTES:

1. SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS.

2. TOP SURFACE TO BE DRESSED BY SAW. REMAINDER MAY BE QUARRY SPLIT.

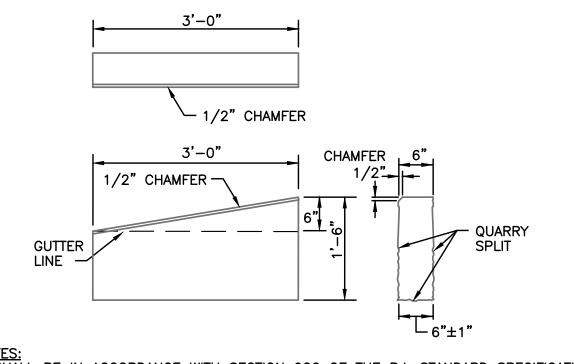
GRANITE 2'-0" RADIUS CORNER STANDARD \ **7.3.4** */* NOT TO SCALE



LIMIT OF BINDER COURSE NOTE:

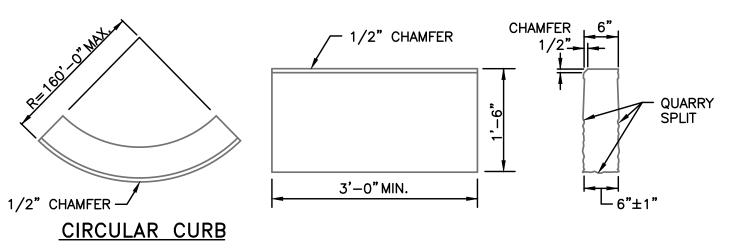
SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS.

BITUMINOUS CONCRETE LIP CURB 7.5.0 NOT TO SCALE



1. SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS. 2. TOP SURFACE TO BE DRESSED BY SAW. REMAINDER MAY BE QUARRY SPLIT.





NOTES:

1. SHALL BE IN ACCORDANCE WITH SECTION 906 OF THE R.I. STANDARD SPECIFICATIONS. 2. TOP SURFACE TO BE DRESSED BY SAW. REMAINDER MAY BE QUARRY SPLIT. 3. MINIMUM LENGTH OF STRAIGHT OR CIRCULAR PIECES TO BE 3'-0".

4. CIRCULAR CURB IS REQUIRED ON CURVES WITH RADII OF 160'-0" OR LESS. STRAIGHT CURB TO BE USED ON CURVES OF MORE THAN 160'-0" RADIUS.

R.I. STANDARD GRANITE CURB 7.3.0 NOT TO SCALE



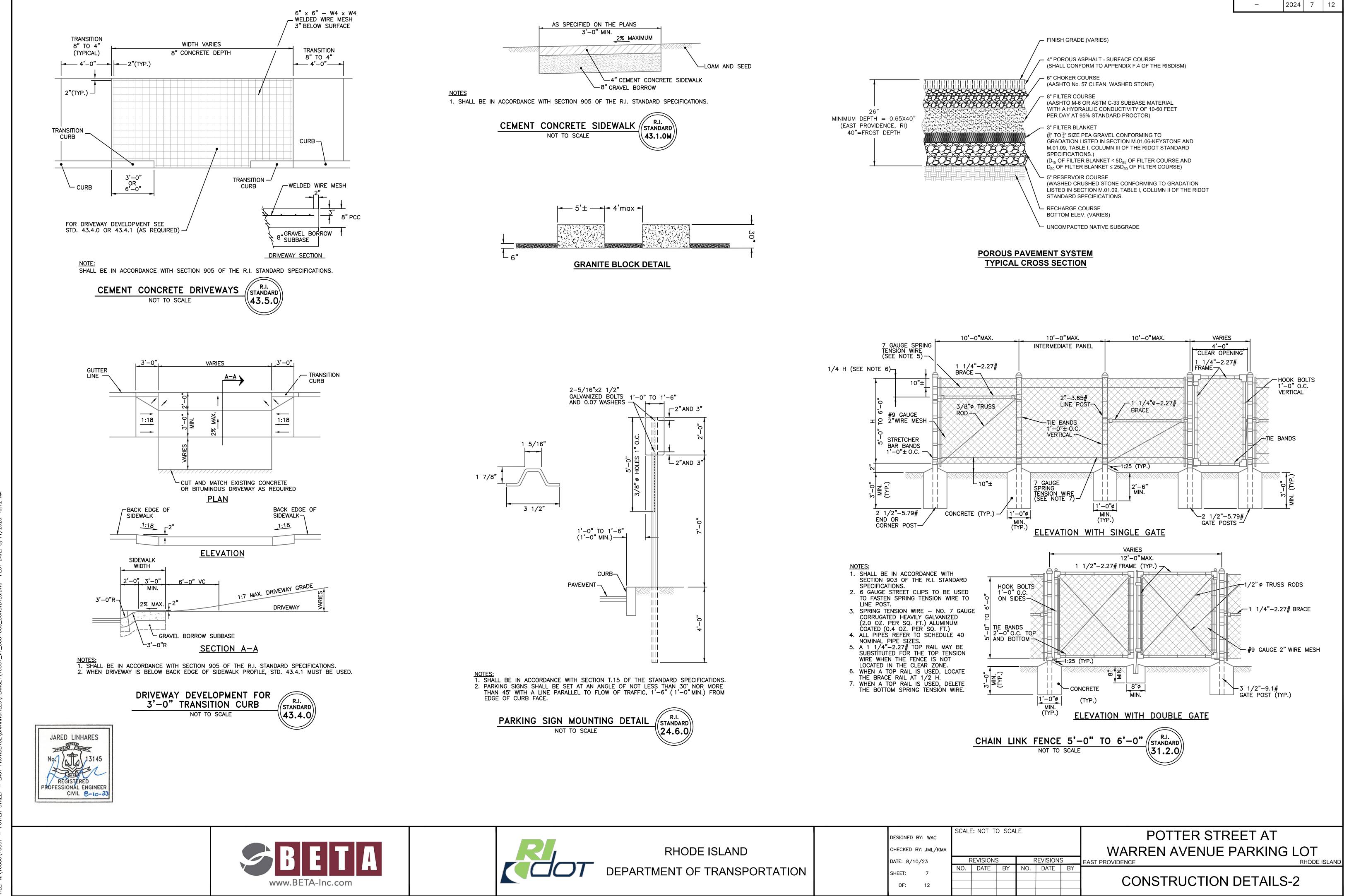


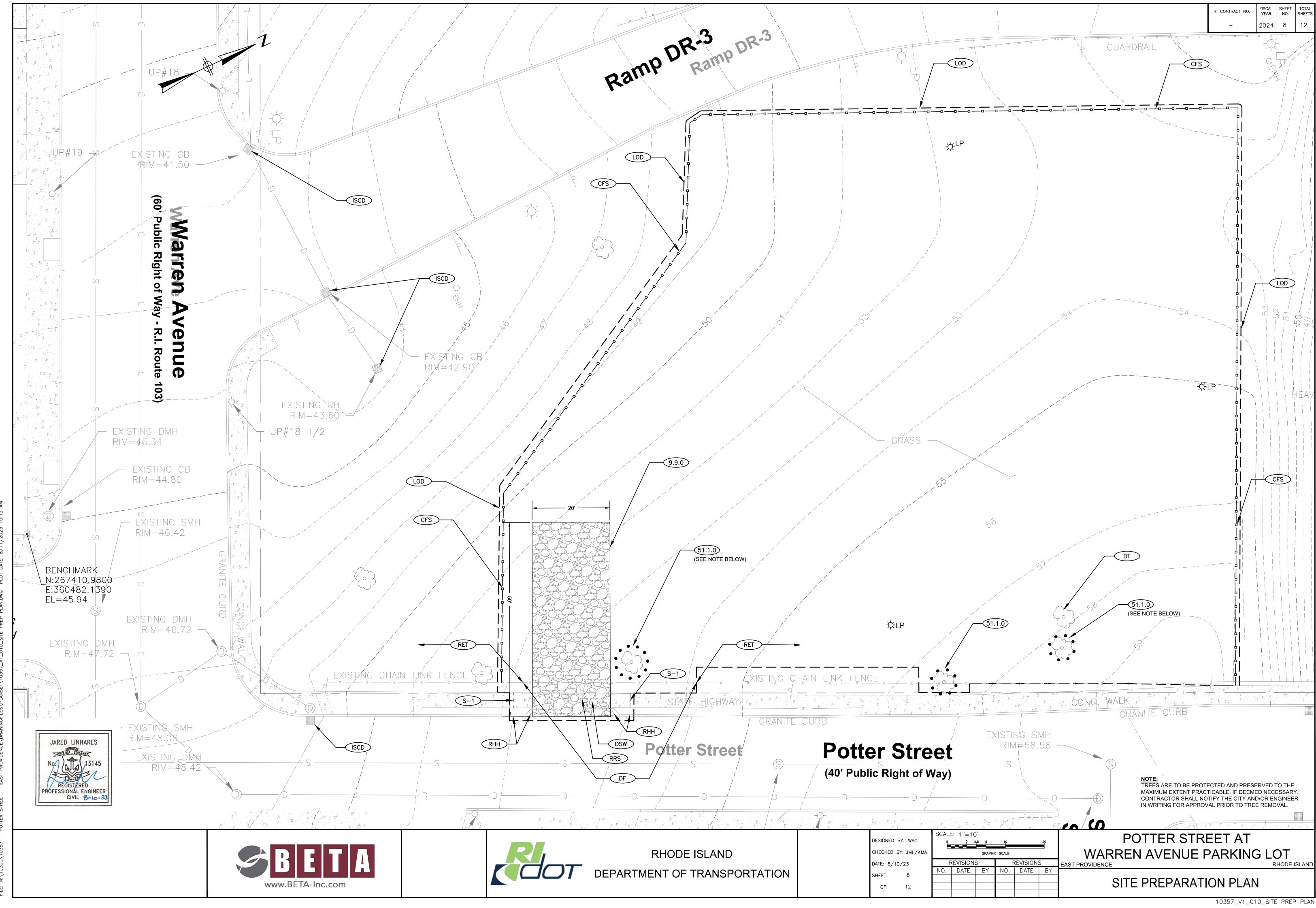


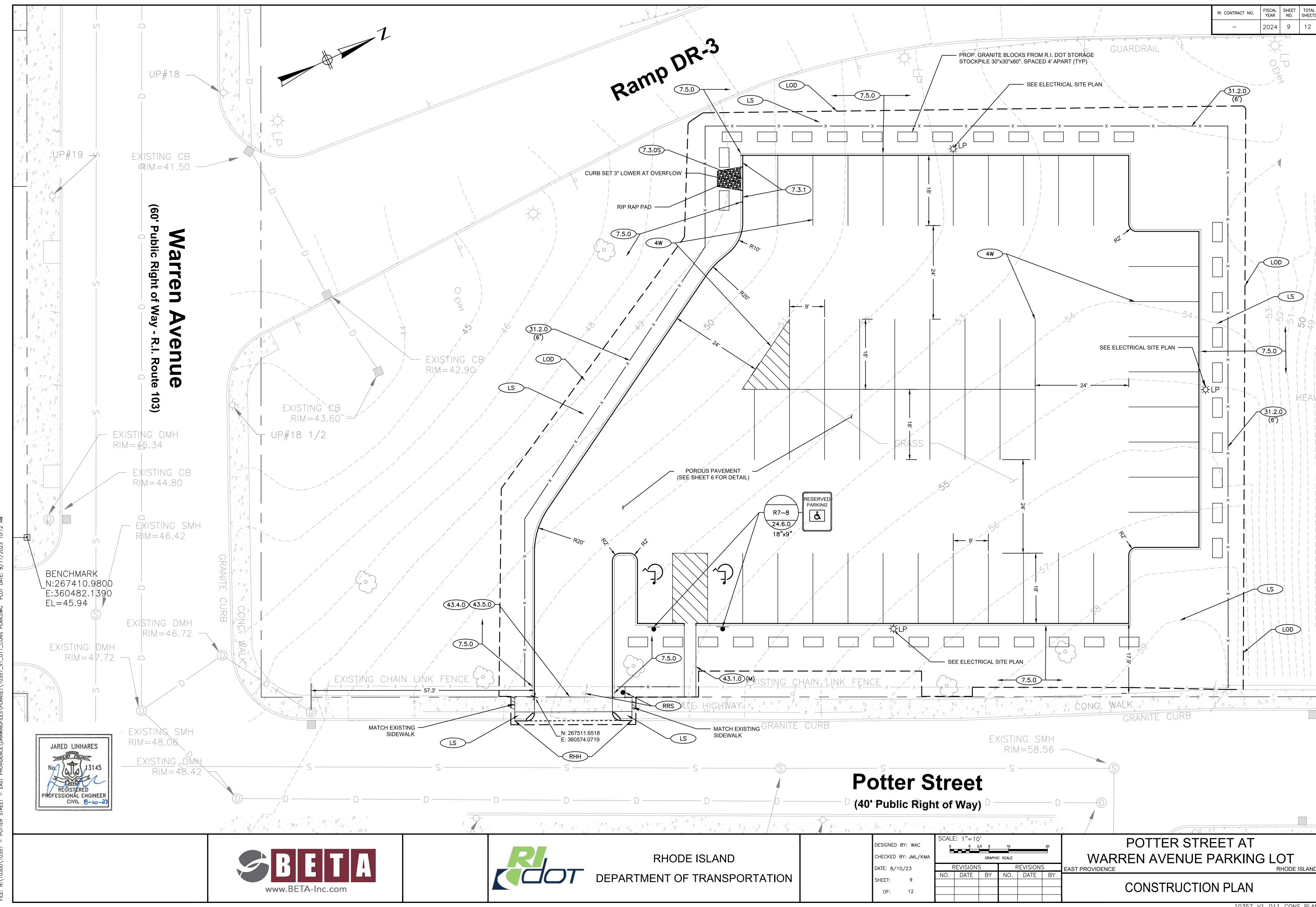
RHODE ISLAND DEPARTMENT OF TRANSPORTATION

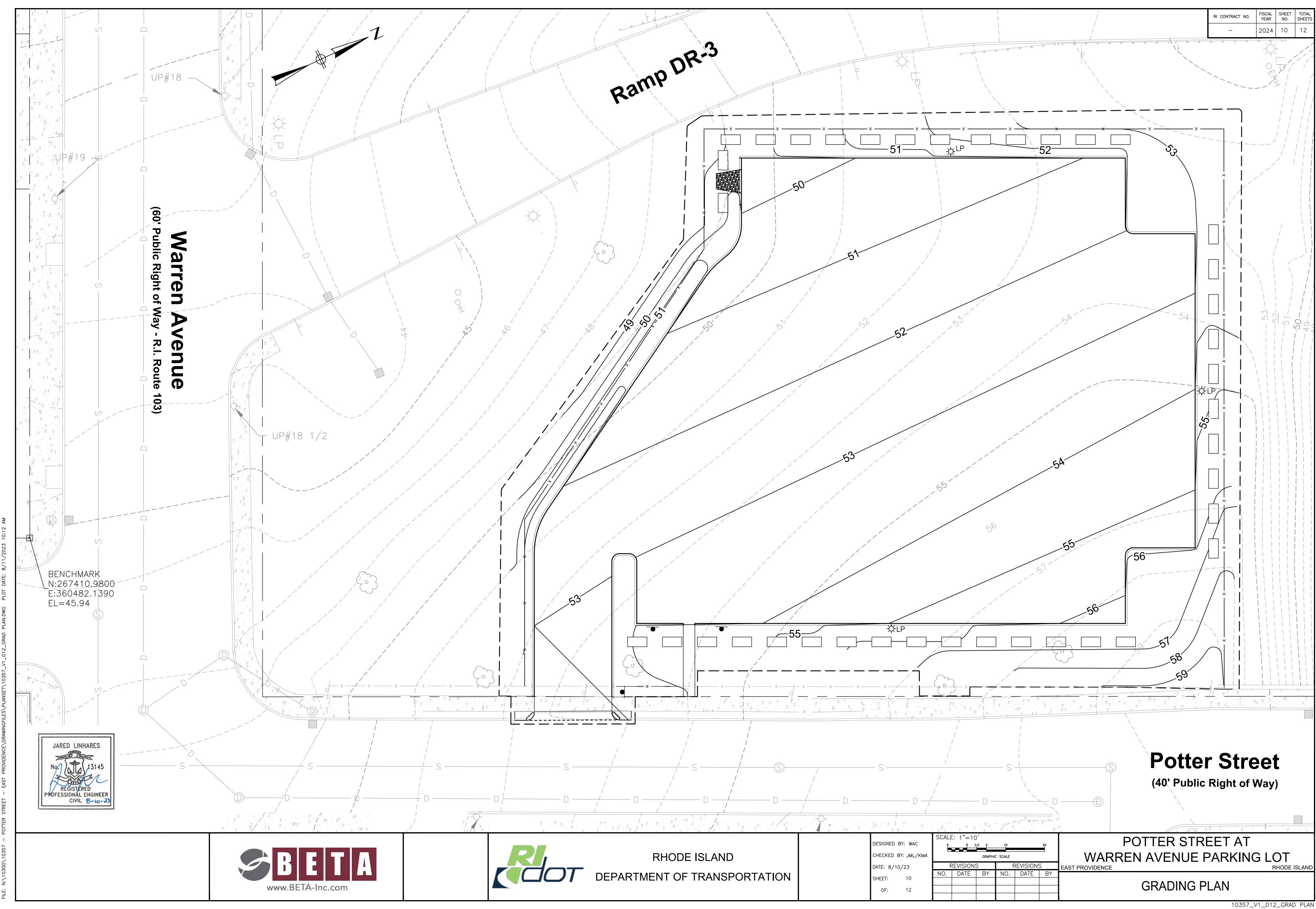
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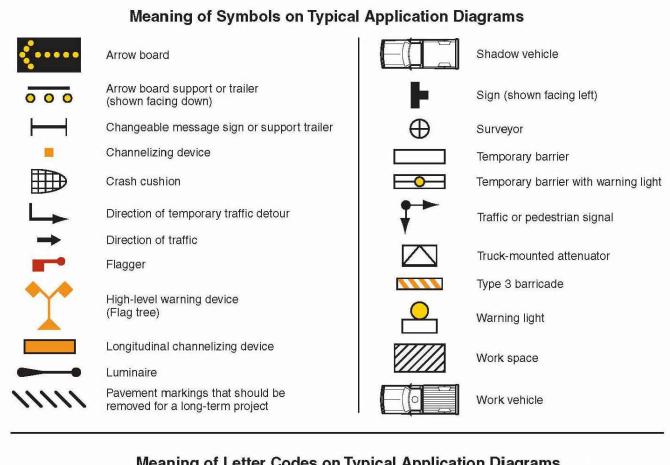
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| EVISIONS | 3 | F | REVISION | S | EAST PROVIDENCE | RHODE IS |
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| | | | | | CONCEDITON DETAILS | 1 |









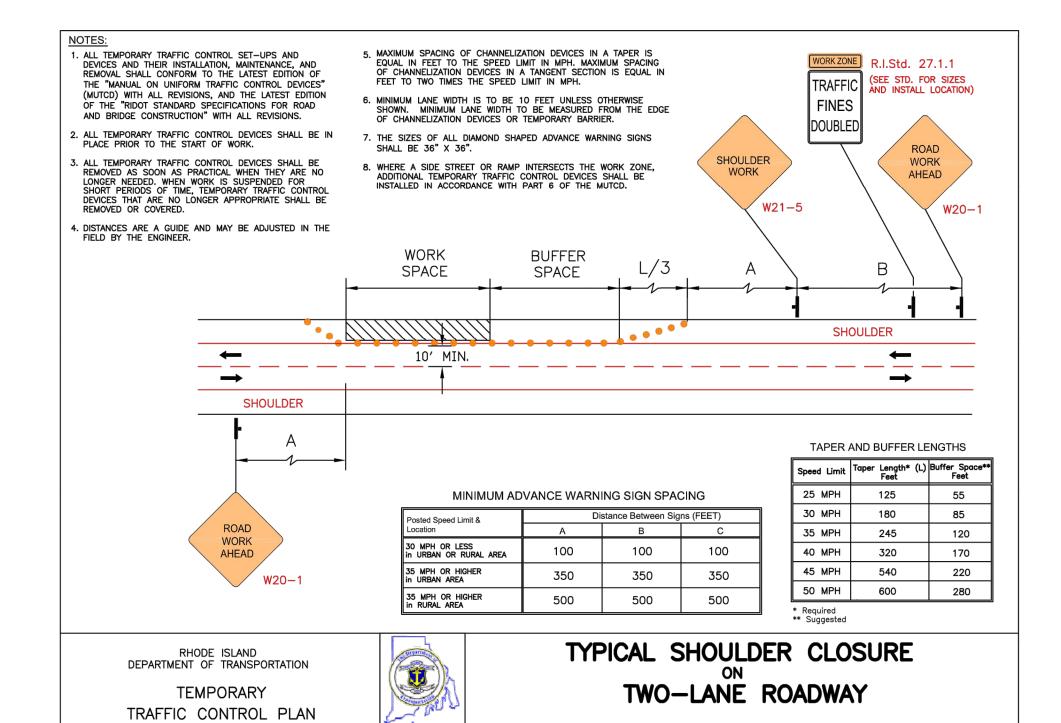


| Dood Type | Dist | ance Between Sig | ns** |
|----------------------|------------|------------------|-----------|
| Road Type | Α | В | С |
| Urban (low speed)* | 100 feet | 100 feet | 100 feet |
| Urban (high speed)* | 350 feet | 350 feet | 350 feet |
| Rural | 500 feet | 500 feet | 500 feet |
| Expressway / Freeway | 1,000 feet | 1,500 feet | 2,640 fee |

** The column headings A, B, and C are the dimensions shown in Figures 6H-1 through 6H-46. The A dimension is the distance from the transition or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the TTC zone. The "third sign" is the sign that is furthest upstream from the TTC zone.)

| Speed (S) | Taper Length (L) in feet |
|----------------|--------------------------|
| 40 mph or less | L = \frac{WS^2}{60} |
| 45 mph or more | L= WS |

Where: L = taper length in feet W = width of offset in feet S = posted speed limit, or off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph



NOT TO SCALE







DATE: 12-23-08

RHODE ISLAND DEPARTMENT OF TRANSPORTATION DESIGNED BY: WAC CHECKED BY: JML/KMA DATE: 8/10/23 SHEET: OF:

SCALE: NOT TO SCALE REVISIONS REVISIONS IO. | DATE NO. DATE

WORK ZONE R.I.Std. 27.1.1

TRAFFIC (SEE STD. FOR SIZES AND INSTALL LOCATION)

WORK

AHEAD

BUFFER LENGTHS

Speed Limit Upstream
Buffer Space*
(Feet)

120

170

220

280

25 MPH 30 MPH 35 MPH

40 MPH

45 MPH

50 MPH

DATE: 12-23-08

W20 - 1

FINES

DOUBLED

W20-4

 \rightarrow

ROAD AHEAD

W20-7a

POTTER STREET AT WARREN AVENUE PARKING LOT

TEMP. TRAFFIC CONTROL PLAN

| | | | | | | Speed L |
|------------------------------|---|--|-------------|-------------------|-------|-----------|
| ROAD | | MINIMUM AD | VANCE WARNI | ING SIGN SPAC | CING | |
| WORK AHEAD ONE LANE ROAD | | Posted Speed Limit & Location | | nce Between Signs | | 30 MF |
| WORK ZONE | N/ | 30 MPH OR LESS in URBAN OR RURAL AREA | 100 | В 100 | 100 | 35 MF |
| W20-1 TRAFFIC W20-4 | ₩20-7a | | | | | 40 MF |
| FINES | | 35 MPH OR GREATER in URBAN AREA | 350 | 350 | 350 | 45 MF |
| DOUBLED | | 35 MPH OR GREATER in RURAL AREA | 500 | 500 | 500 | 50 MF |
| R.I.Std. 27.1.1 | | | | | | * Suggest |
| RHODE ISLAND | The Desirement of the Land of | | TYDIC | AL LAN | F CLO | CLIDE |
| DEPARTMENT OF TRANSPORTATION | | | | ON ON | | SUIL |
| TEMPORARY | | | TWO | -LANE | ROAD | WAY |
| TRAFFIC CONTROL PLAN | 2 1780 2 | | | | | |
| | 8 | NOT TO SCA | ALE | | | DAT |
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1. ALL TEMPORARY TRAFFIC CONTROL SET-UPS AND DEVICES AND THEIR INSTALLATION, MAINTENANCE, AND REMOVAL SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES"

6. MAXIMUM SPACING OF CHANNELIZATION DEVICES IN THE 100' MAX. TWO-WAY TRAFFIC TAPERS IS 25 FEET. MAXIMUM SPACING OF CHANNELIZATION DEVICES IN A TANGENT SECTION IS EQUAL IN FEET TO TWO TIMES THE SPEED LIMIT IN MPH.

7. MINIMUM LANE WIDTH IS TO BE 10 FEET UNLESS OTHERWISE SHOWN. MINIMUM LANE WIDTH TO BE MEASURED FROM THE EDGE OF CHANNELIZATION DEVICES OR TEMPORARY BARRIER.

8. THE SIZES OF ALL DIAMOND SHAPED ADVANCE WARNING SIGNS SHALL BE 36" X 36".

9. WHERE A SIDE STREET OR RAMP INTERSECTS THE WORK ZONE, ADDITIONAL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED IN ACCORDANCE WITH PART 6 OF THE MUTCD.

WORK

SPACE

10' MIN.

UPSTREAM

SPACE

BUFFER

MAX.

DOWNSTREAM

SPACE

BUFFER

(MUTCD) WITH ALL REVISIONS, AND THE LATEST EDITION OF THE "RIDOT STANDARD SPECIFICATIONS FOR ROAD AND

2. ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE IN PLACE PRIOR TO THE START OF WORK.

3. ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER NEEDED. WHEN WORK IS SUSPENDED FOR SHORT PERIODS OF TIME, TEMPORARY TRAFFIC CONTROL DEVICES THAT ARE NO LONGER APPROPRIATE SHALL BE REMOVED OR COVERED.

4. DISTANCES ARE A GUIDE AND MAY BE ADJUSTED IN THE FIELD BY THE ENGINEER.

NECESSARY SO THAT THE 100' MAX. TWO-WAY TRAFFIC TAPERS ARE PLACED BEFORE HORIZONTAL (OR CREST

VERTICAL) CURVES TO PROVIDE ADEQUATE SIGHT DISTANCE FOR THE FLAGGERS AND QUEUES OF STOPPED VEHICLES.

5. THE BUFFER SPACES SHOULD BE EXTENDED IF

BRIDGE CONSTRUCTION" WITH ALL REVISIONS.